

Community Development Department
Building • Planning • Code Enforcement

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 Woodland, WA 98674
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STAFF REPORT & RECOMMENDATION
Port of Woodland
Guild Road Industrial Park Phase I

ISSUE DATE: December 28, 2018

Land Use Application No.: SPR2018-004/CAP2018-001

Applicant:	Port of Woodland 1608 Guild Road Woodland, WA 98674
Property Owner:	Port of Woodland 1608 Guild Road Woodland, WA 98674
Site Location:	1435 Guild Road Woodland, WA 98674
Parcel Number(s) & Size:	508310100. Approximately 12.01 acres
Zoning Designation:	Light Industrial (L-1)
Application Received:	June 21, 2018 Last submittal July 19 th , 2018
Notice of Application & Likely DNS Issued:	August 14, 2018 (SEPA completed by Port of Woodland June 22, 2018)
Published:	Not required per WMC 19.06.

I. DESCRIPTION OF PROPOSAL:

The Port of Woodland proposes development of an industrial park including six (6) new buildings ranging in size from 15,000 to 25,000 square feet. The project includes construction of a portion of a new public street, off-site sewer extension and a stormwater treatment facility.

II. LOCATION OF PROPOSED DEVELOPMENT

The property is located on the south side of Guild Road at 1435 Guild Road, Woodland, Washington: Parcel numbers, 508310100, owned by the Port of Woodland. The site is in Township 5 North, Range 1 West, Section 14, Willamette Meridian.

III. REVIEW AUTHORITY

Per WMC 19.08.030, Type II Site Plan Reviews shall be approved, approved with conditions, or denied by the Development Review Committee.

IV. AGENCY AND PUBLIC COMMENTS

Washington State Department of Archaeology and Historic Preservation (DAHP) – Comments from DAHP were received wherein they requested that a professional archaeological survey be complete. Consultants for the applicant had already performed such a survey and the results of that work was distributed to DAHP. After review of the survey report, no additional comments were received, and no conditions of approval were recommended for the proposal.

Washington State Department of Ecology (DOE) – DOE commented that erosion control measures were warranted; that development activities would need to comply with stormwater requirements; and that solid waste measures (like the use of clean fill and proper debris removal) would be required.

Staff Response: Staff recognizes that these standards are triggered, and that the City will review these standards under existing City codes as discussed below. No additional conditions of approval are warranted.

V. FINDINGS - DEVELOPMENT STANDARDS COMPLIANCE

Zoning (Chapter 17.44 – Light Industrial)

Permitted Uses (Section 17.44.020)

Finding 1: The I-1 district allows for a range of permitted uses. The proposal involves the construction of buildings and supporting infrastructure for future occupation of business that are unknown currently. Tenant uses will be required to be a permitted use unless the tenant completes a conditional use permit review or other land use review prior to occupancy of the proposed buildings.

Conclusion: The proposed structures used appropriately are permitted in this zone.

Minimum Area (Section 17.44.040)

Finding 2: There is a ten (10) acre minimum area for the I-1 zoning district. The project site is approximately 12.01 acres in size and part of a larger light-industrial area.

Conclusion: The proposal complies with this standard.

Minimum Lot Size (Section 17.44.050)

Finding 3: There is a ten-thousand (10,000) square foot minimum lot size for the I-1 zoning district. The project site is approximately 12.01 acres in size and no further division of the property is proposed at this time.

Conclusion: The proposal complies with this standard.

Minimum Lot Width and Depth (Section 17.44.060)

Finding 4: Minimum lot width is sixty-five (65) feet as measured along the front property line, and there is no minimum lot depth. The lot width is more than sixty-five (65) feet and nothing in the proposal will change the lot width.

Conclusion: The proposal complies with this standard.

Building Setbacks (Section 17.44.070)

Finding 5: The minimum front setbacks for all buildings shall be twenty-five feet. Side setbacks for all buildings shall be ten feet; and provided where the I-1 zone abuts a residential zone, the side yard setback shall be a minimum of twenty-five feet. The minimum rear yard setback for all buildings shall be ten feet; provided where the I-1 zone abuts a residential zone, the side yard setback shall be a minimum of twenty-five feet. (There is no residential zone adjacent to the proposed project.) The buildings proposed at this time are all consistent with the setbacks for the I-1 zone.

Conclusion: As proposed, the project meets setback standards.

Building Height (Section 17.44.080)

Finding 6: I-1 use buildings on lots sized one acre or less shall be no more than three stories high or exceed forty-five feet in height. I-1 use buildings on lots greater than one acre shall be no more than fifty-five feet to eave height. Additionally, there are exceptions to the building height standard (cranes etc.). The buildings as proposed are 35 feet in height.

Conclusion: As proposed, the project complies with this standard.

Lot Coverage (WMC 17.44.090)

Finding 7: There are no limitations for lot coverage except where the I-1 use abuts a residential zone. Since this project does not abut a residential zone, no lot coverage limits apply to this project.

Conclusion: The proposal complies with this standard.

Off Street Parking and Loading (WMC 17.44.100)

Finding 8: Parking must be consistent with WMC 17.56. See below.

Conclusion: The proposal complies with this standard.

Vehicular Access (WMC 17.44.120)

Finding 9: I-1 properties have limits on the type of roads they may use for access (arterials). The proposed project does not involve the designation of new I-1 property so the location of it on the existing road, which is now designated as a Major Collector, is problematic (as it is throughout the City). This problem is the result of the adoption of a map reclassifying the functional classes of the roads within the City (without making the correction to this code section). The site fronts on Guild Road which was designated as an arterial until the reclassification was completed. Dike Access was constructed to Arterial standards. Guild Road fronting this property was recently improved to arterial standards and the applicant will be completing additional improvements to bring the road to the west to City standards as part of the Guild Road Industrial Park Phase 2 project.

Given this, staff points out that the most important section of this standard is that industrial use traffic will not be directed through residential areas. The site will access Guild Road, Schurman Way, Dike Access, and I-5 to the north. None of this route is through a

residential area. Traffic to the east would use Guild Road, which becomes West Scott Road, and then Pacific (which is an arterial) to the point where it meets I-5 at Exit 21. Traffic south would use Guild Road and North Pekin Road before it reaches Goerig Road and crosses the train tracks towards I-5. At that time, the industrial traffic would follow the signed truck route through downtown Woodland. Where there are residential uses along the truck route, staff assumes that the truck route was specifically chosen as the route most likely to minimize traffic impact to the City. To prohibit industrial uses which send traffic to the truck route would seem to be inconsistent with the application of this code section.

Conclusion: Staff does not find the proposal inconsistent with this standard because no new industrial land is being designated where it does not exist removed from an arterial on the new transportation map. Staff also finds that the potential for this development to send trucks to the residential area nearly a mile south of the site, via an adopted truck route, is not sufficient grounds to find the proposal inconsistent with this standard.

Title and Purpose (WMC 17.44.130)

Finding 10: This section is mistitled. The content of this section seems to be the start of the landscaping section so please refer to that analysis below.

Landscaping (WMC 17.44.130 through .138)

Finding 11: WMC 17.44.130 through .138 are triggered by the proposal.

Finding 12: The applicant has submitted the landscaping plan material with the application material as required by WMC 17.44.133 and .134. except it was not stamped and signed by a qualified professional.

Finding 13: The proposed master landscaping plan appears to comply with the standard of WMC 17.44.135 in terms of: landscaping materials; shrub sizing and spacing; tree size and spacing; and non-plant ground covers. For additional comments see below. Implementation of the landscaping will be required prior to occupancy of the first building on the site. (See Site Plan Condition #15)

Finding 14: The applicant has submitted a master landscaping plan with the application material as required by WMC 17.44.137. That plan shows that the proposal will have a conformity of landscaping style throughout the site. A requirement that this plan be part of the covenants has been added as a condition of approval. (See Site Plan Condition #13)

Finding 15: Ten percent of the site is required to be landscaped. The landscaping plan shows 17% of the percentage will be landscaped.

Finding 16: Landscaping Required Setbacks - WMC 17.44.136(A) through (D) require that setbacks be landscaped. The plan shows setbacks to be landscaped to standard.

Finding 17: Landscaping in Parking Areas - WMC 17.44.136(F) requires that 10% of parking and maneuvering areas be landscaped. This landscaping area shall be evenly distributed throughout with shade trees and shrubs/groundcover including the use of landscape islands. The proposed landscaping plan meets this standard.

Finding 18: Landscaping plant selection as shown on the plan includes several species that are not native to the area AND includes some species that can be considered invasive species. Normally, each plant selection includes language that allows for an "approved equal" but this plan does not include that notation. Invasive species should not be included in the planting plan. (See Site Plan Condition #14)

Finding 19: Requirements for Maintenance and Irrigation. The use of drought-tolerant plant species is encouraged and shall be required when irrigation is not available. Irrigation shall be provided for plants that are not drought-tolerant. In this case, note 11 from Page 13 of the plans clearly indicates that irrigation will be provided by others.

If the plantings fail to survive, the property owner shall replace them with an equivalent specimen (i.e., evergreen shrub replaces evergreen shrub, deciduous tree replaces deciduous tree, etc.). All man-made features required by this chapter shall be maintained in good condition, or otherwise replaced by the owner. The property owners shall maintain any landscape materials required by this chapter in a way that they do not adversely impact the usage of any off-site solar panels and windmills that exist at the time of issuance of the landscaping plan approvals.

A condition of approval has been added requiring that the maintenance and irrigation standards be met for the site. (See Site Plan Condition #16)

Conclusion: The landscaping standards can be met as conditioned.

Lighting (17.44.140)

Finding 20: A lighting plan was not included in the plan. A lighting plan shall be submitted and approved by the Public Works Director prior to selection of lights. LED lighting shall be used. Lighting shall be designed and arranged to not reflect or cause glare; rotate; glitter; flash; or conflict with the readability of traffic signs or control signals. The operator shall be responsible for ensuring that lighting is installed and arranged to ensure that no reflection or glare shall conflict with the readability of traffic signs or control signs. Signage shall also not rotate, glitter or flash per WMC 17.46.140. (See Site Plan Conditions #1 & #20)

Conclusion: As conditioned the proposal can comply with these standards.

Site Standards (17.44.160)

Finding 21: Building and Yard Maintenance - Developments in the I-1 zoning district are required to maintain their buildings and yards in a neat and orderly manner including keeping landscaping in a healthy and presentable state. This includes keeping structures, buildings, fences and walls free of rust, corrosion, peeling paint and surface deterioration. Building facades shall also meet the standards included in the code.

A condition of approval regarding these standards has been added for informational purposes but their application will occur at a future time. (See Site Plan Condition #16)

Conclusion: As conditioned the proposal can comply with these standards.

Industrial off-site improvements (17.44.200 through .240)

Finding 22: Public facilities and services necessary to support proposed developments must be in place (or provided in a timely manner) to meet required standards. In this case, the applicant is proposing a development that triggers additional public improvements and the plans meet the standards as outlined herein and as conditioned.

Conclusion: As conditioned this project can comply with these sections.

Chapter 17.48 – Performance standards

Finding 23: The requirements of this section cover hazards and nuisances including sound level, vibration, air emissions, smoke, dust, odors, industrial wastes, fire hazards, heat, glare, radioactivity and radio transmitters.

The applicant will be responsible for ensuring that their operation is operating with all local, state and federal nuisance laws. Nothing in the applicant's submittal show a need for concern over potential for high levels of these hazards or nuisances. Nothing has been raised to the attention of staff that indicates concerns that are not already covered by existing law that the applicant must comply with. A condition of approval regarding these standards has been added for informational purposes until specific site plan and building permit applications are received. (See Site Plan Condition #16)

Conclusion: As conditioned the proposal can comply with this standard.

Streets and Sidewalks (Title 12)

Finding 24: Street improvements will be required along the frontage at Guild Road. The Port improved this street to City standards for a portion of the frontage in 2011. It appears that proposed right of way dedications from that project were not finalized or recorded. Additional half-street frontage improvements are required. The design basis for Guild Road shall match the existing improved portion of Guild and shall be consistent with City of Woodland Standard Drawing T-25A for Commercial/Industrial Collector including lighting. Half-street frontage improvements and right-of-way dedication to City standards along the west side of the project is also required per the proposed plan. Construction and right-of-way dedication will need to be finalized prior to occupancy for any of the buildings on site. (See Site Plan Conditions #11 & #12)

Conclusion: The proposal can comply with the development standards.

Water and Sewage (Title 13)

Finding 25: Water and Sewer mains are required for the new frontage road. Water main shall be 12-inch diameter. Gravity Sewer shall be 12-inch diameter. Water and sewer mains including services to property and fire hydrant installation shall comply with Woodland Development Standards. Connection charges for water and sewer services shall be per the adopted rate schedule at the time of connection.

Industrial building connections shall require backflow devices in accordance with state regulations and Woodland Municipal Code. Any industrial or large flow discharges to the

waste system will require additional review for water quality and/or quantity impact assessment.

Conclusion: The proposal can comply with the development standards.

Erosion Control Ordinance (Chapter 15.12)

Finding 26: Applicant is required to install and maintain erosion control measures per the Best Management Practices as outlined in this section (2012 Stormwater Management Manual for Western Washington) during site excavations and grading. An NPDES permit from the Department of Ecology is required where more than one acre is being disturbed.

A preliminary erosion control plan was not submitted as part of the application package but will be required as part of the final engineering plan submittal prior to construction. (See Site Plan Condition #19)

Conclusion: As conditioned, the proposal can comply with City standards. Submit final erosion control plans and demonstrate compliance with DOE standards for each phase of site development.

Stormwater Management (Chapter 15.12)

Finding 27: The applicant's submittal included a preliminary stormwater TIR. Applicant is proposing detention ponds for quantity control and treatment by use of wet pond BMP's in accordance with WMC 15.12 and city engineering standards. The basis of the design is to treat the onsite water, plus the water from the past Guild Road widening. The treatment and detention are designed to accommodate that development.

Conclusion: The proposal can comply with the development standards.

Off-Street Parking (Chapter 17.56.040)

Finding 28: Parking and loading facilities shall be located at the side or rear of buildings; provided, that necessary parking and loading may be permitted at the front only when appropriately landscaped according to the standards set out in Chapter 17.44.

In this case, the proposal is designed around an internal parking and loading area that will be largely screened from Guild Road. However, there is a small parking area that will be visible from the street being constructed on the west side of the site (Howard Way). Staff finds this design can meet the intent of the standard as shown once the landscaping plan is revised to include plant selections that are native to the northwest. (See the section on landscaping.)

Finding 29: Parking is related to the specific size of building constructed, and the use of that building once it is occupied. Staff has reviewed the applicant's submittal which included a parking plan requiring 156 parking spaces. The proposed plans show up to 163 parking spaces as currently configured. This includes 10% (17) compact spaces and 12 ADA spaces. Staff has reviewed the number of parking spaces and is confident that the overall industrial park can meet its parking needs in the future.

Finding 30: Isle widths, loading standards, parking space sizes, and the number of ADA parking spaces meet the standards of WMC 17.56. The number and location of the 12 proposed ADA parking spaces is adequate.

Conclusion: As conditioned, the proposal can comply with the off-street parking standard.

Sign Requirements (Chapter 17.52)

Finding 31: Signage is not proposed as part of this application. Future signage will require separate review and building permits and must meet WMC 17.52.080.

Conclusion: As proposed the project should comply with these standards.

General Review comments

Finding 32: Fire Impact Fees – Per WMC 3.41, the City of Woodland assesses Fire Impact Fees on new development. The Fire Impact Fee is \$0.51 per square foot of building space. Fire impact fees will be assessed for each building as shown on the site plan:

Building 1 – 15,000 sq. ft. x \$.51 per sq. ft. = \$7,650.00

Building 2 – 23,000 sq. ft. x \$.51 per sq. ft. = \$11,730.00

Building 3 – 23,000 sq. ft. x \$.51 per sq. ft. = \$11,730.00

Building 4 – 25,000 sq. ft. x \$.51 per sq. ft. = \$12,750.00

Building 5 – 25,000 sq. ft. x \$.51 per sq. ft. = \$12,750.00

Building 6 – 15,000 sq. ft. x \$.51 per sq. ft. = \$7,650.00

For a project total of \$64,260 in Fire Impact Fees. Impact fees will be assessed and collected at the time of building permit issuance. (See Site Plan Condition #10)

Finding 33: Transportation Impact Fees (TIF) are required per WMC 3.42 on new development to support future transportation improvements within the city. The TIF is calculated based on \$838 per PM peak hour trip (PMPHT) generated by the project based on the project Traffic Study or where no study is prepared, based on trip generation in accordance with Institute of Transportation Engineers (ITE) published data. For this proposal, Port of Woodland prepared a Transportation Impact Analysis Report (Global Transportation Engineering, April 19, 2018). The recommended trip generation is based on the ITE manual for the Light Industrial Classification (ITE Code 110). The classification calls for 0.97 peak hour trips per 1,000 square feet for the proposed industrial buildings. The transportation impact fee (TIF) will be collected at the time of building permit issuance for each building. An example TIF calculation is shown below:

The memo calculates 126,000 square feet of light industrial peak hour trips calculated as follows: 126 (1,000 square feet) x 0.97 (ITE number for peak hour trips) or 122.22 new peak hour trips. The number of peak hour trips results in a calculated Transportation Impact Fee of (122.22 X \$838) = \$102,420.36 for the project.

Impact fees are collected at the time of building permit issuance. A condition of approval has been added which requires the fees be paid at the time of building permit issuance. (See Site Plan Condition #10)

Finding 34: Fire Department Access – Fire department key box for building access and approved lock or switch gate access must be provided per IFC 503.5.1 and 506. Approved Fire Apparatus Access Roads or Drive Aisles must be provided within 150’ or all portions of all buildings. The proposed buildings #4 and #5 have inadequate access around the perimeter of the building so the addition of a fire lane is warranted. IFC 503.1.1 (See Site Plan Condition #2)

Finding 35: Fire Hydrants are clearly indicated on the plans provided. Hydrants must be provided in accordance with the IFC/Washing Fire Code based on building size and required fire flow. Typically, hydrants must be provided along any required fire apparatus access roads with spacing no greater than 500’ and no point on required access roads to be farther than 250’ from the closest hydrant. The proposed on-site hydrants are acceptable to the fire marshal. However, there are no hydrants shown on the plans for the road being built on the west side of the site (Howard Way). Hydrants must be provided along this improved roadway. (See Site Plan Condition #3)

Finding 36: The Fire Department Connection shall have a 5” storz type connection with 30-degree bend and ball drip drain as specified by Clark County Fire & Rescue. (See Site Plan Condition #4)

Finding 37: The Fire Department Access on the South Side of Buildings 4 & 5 must be 20’ wide, providing access to within 150’ of any point on the perimeter of the building. (See Site Plan Condition #5)

Finding 38: Buildings heights exceed 30’ so access must be configured to provide aerial apparatus access on at least one side of any building exceeding 30’ in height in accordance with the IFC appendix D. (See Site Plan Condition #6)

Finding 39: Fire Department Connections for building fire suppression systems must be located remote from the buildings and within 150’ of a Fire Hydrant. (See Site Plan Condition #7)

Finding 40: Address Numbers for the complex and individual buildings will need to be clearly visible from the public street as well as the on-site access areas. (See Site Plan Condition #8)

Finding 41: The application packet included a Cultural Resource Survey completed by Archaeological Services. The survey concluded that the proposed project will have no effect on historic properties and recommended that no further cultural resource work was needed within the project area. A copy of this report was sent the Washington State Department of Archaeology and Historic Preservation (DAHP) which did not respond with any concerns about the report or its findings.

Finding 42: A Geotechnical Site Investigation report was included with the application material. This report includes recommendations for the development that are not based in WMC. Staff recommends that these recommendations be implemented throughout the development.

Finding 43: The applicant had an underground storage tank assessment done which showed that there had been two tanks on the site. One had been removed and the second has been "closed in place". No additional actions are required at this time.

Finding 44: Critical Area Permit – The applicant submitted a critical area report with the application material which found that there was both wetland and wetland buffer on site. The report includes a recommendation that impacts be mitigated through the purchase of credits from a mitigation bank. Proof that the mitigation credit purchase has been completed will be required prior to occupancy for any building on the site. (See Site Plan Condition #9)

VI. SEPA DECISION:

The Port of Woodland is acting as the lead agency and has reviewed the proposed project for probable adverse environmental impacts and issued a determination of non-significance (DNS) for this project on June 22nd, 2018. The proposal may include mitigation measures under applicable codes, and the project review process may incorporate or require mitigation measures regardless of whether an Environmental Impact Statement is prepared. A copy of the subsequent threshold determination for the proposal may be obtained from the SEPA responsible official upon request.

Jennifer Keene, Executive Director
Port of Woodland
1608 Guild Road
Woodland, WA 98674

Email: jkeene@portofwoodland.com
Phone: 360-225-6555

VII. DECISION:

Per WMC 19.08.030, the above application for preliminary Site Plan Review has been **approved with conditions** by the City based on the criteria and standards outlined in Woodland Municipal Code (WMC).

VIII. CONDITIONS OF APPROVAL:

Site Plan:

1. The operator shall be responsible for ensuring that lighting is installed and arranged to ensure that no reflection or glare shall conflict with the readability of traffic signs or control signs. Lighting shall also not rotate, glitter or flash per WMC 17.46.140. Submit a manufacturer's lighting cut sheet for all proposed outdoor lights.
2. Fire Department Access – Fire department key box for building access and approved lock or switch gate access must be provided per IFC 503.5.1 and 506.
3. Fire Hydrants are clearly indicated on the plans provided. Hydrants must be provided in accordance with the IFC/Washington Fire Code based on building size and required fire flow. Hydrants must be constructed along the street on the west side of the project at a placement spacing for the additional hydrant(s) coordinated with Clark County Fire & Rescue.
4. The Fire Department Connection shall have a 5" storz type connection with 30-degree bend and ball drip drain as specified by Clark County Fire & Rescue.
5. The Fire Department Access on the South Side of Buildings 4 & 5 must be 20' wide, providing access to within 150' of any point on the perimeter of the building.

6. Provide aerial apparatus access on at least one side of any building exceeding 30' in height in accordance with the IFC appendix D.
7. Fire Department Connections for building fire suppression systems must be located remote from the buildings and within 150' of a Fire Hydrant.
8. Address Numbers for the complex and individual buildings will need to be clearly visible from the public street as well as the on-site access areas.
9. Proof that the purchase of wetland mitigation bank credits has been completed will be required prior to occupancy for any building on the site.
10. Impact fees for Fire and Transportation are due and payable at the time of building permit issuance for each building. Actual impact fees will be calculated based on the individual building and site plan applications at the time of the development submittal:
 - a. Fire Impact Fees:
 - i. \$.51 per square foot – 126,000 sf of new light industrial = \$64,260
 - b. Transportation Impact Fees:
 - i. 0.97 peak hour trips per 1,000 sf, \$838 per trip – 126,000 sf of new light industrial: $0.97 \times 126 \times \$838 = \$102,420.36$

Water and sewer assessment will be based on the current rate schedule at the time of building permit.

11. Improve Howard Way in accordance with the findings for Title 12 above and to match the approved plans. Water and sewer mains, each 12-inch diameter pipelines, shall be completed within the Howard Way extension and the new frontage road in accordance with the application drawings.
12. Improve Guild Road half-width to Commercial/Industrial Collector Standards. Dedicate all right of way. Extend water and sewer mains, each 12-inch diameter, as necessary.
13. WMC 17.44.137 requires that covenants for a master landscape plan shall be recorded as part of a site plan in the light industrial zone. Submit proposed covenant documents for review prior to approval occupancy of the first building on the site.
14. The master landscape plan shall be modified as necessary to ensure that no invasive species are included in the plantings.
15. Landscaping shall be installed prior to any building on the site being occupied.
16. Property owners will be responsible for maintaining the site in accordance with WMC 17.44.130, WMC 17.44.160, and WMC 17.48.
17. Submit final stormwater TIR (technical information report) for approval by the Public Works Director.
18. A draft covenant for storm facility maintenance and access for inspection document shall be submitted for review prior to approval of the final engineering for the site plan. Format for the covenant document can be requested from Woodland Public Works.
19. An erosion control plan will be required as part of the final engineering plan submittal prior to construction. Implementation of the approved plan and compliance with DOE standards is required during construction.
20. Lighting shall be directed or shielded so as not to cast glare onto adjacent properties or roadways. Submit a manufacturer's lighting cut sheet for all proposed outdoor lights.
21. Payment shall be made to the City for any outstanding Professional Consulting Services per Woodland Municipal Code Ordinance 1097.
22. Submit three (3) copies of full-sized and one (1) copy of reduced size (11" x 17") as-built drawings. In addition, submit an electronic version of as-built plans including AutoCAD and .pdf formats.

APPEAL PROCEDURE:

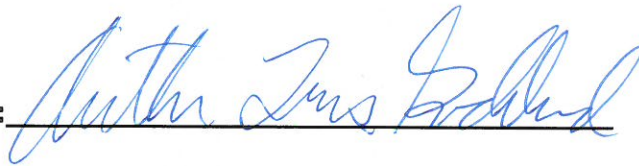
As per WMC 19.06.040, 19.08.030 and 19.10.090, this Decision may be appealed to the Hearing Examiner within fourteen (14) days of the date this decision is issued. The appeal must be made in writing and include the name and address of the party/agency filing the appeal; identify the specific issue(s) being appealed; and a statement of the grounds or reasons for the appeal. Written appeals must also be submitted with the associated appeal fee and a signed reimbursement agreement to be considered complete.

Appeals must be submitted to the City no later than 5:00 P.M. on January 11, 2019.

Responsible Official: Travis Goddard, Community Development Director
City of Woodland
PO Box 9
230 Davidson Ave
Woodland, WA 98661
goddardt@ci.woodland.wa.us

Date: 12/28/2018

Signature:

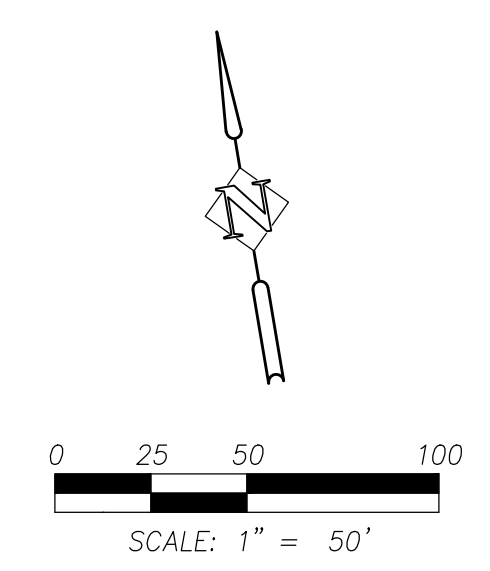
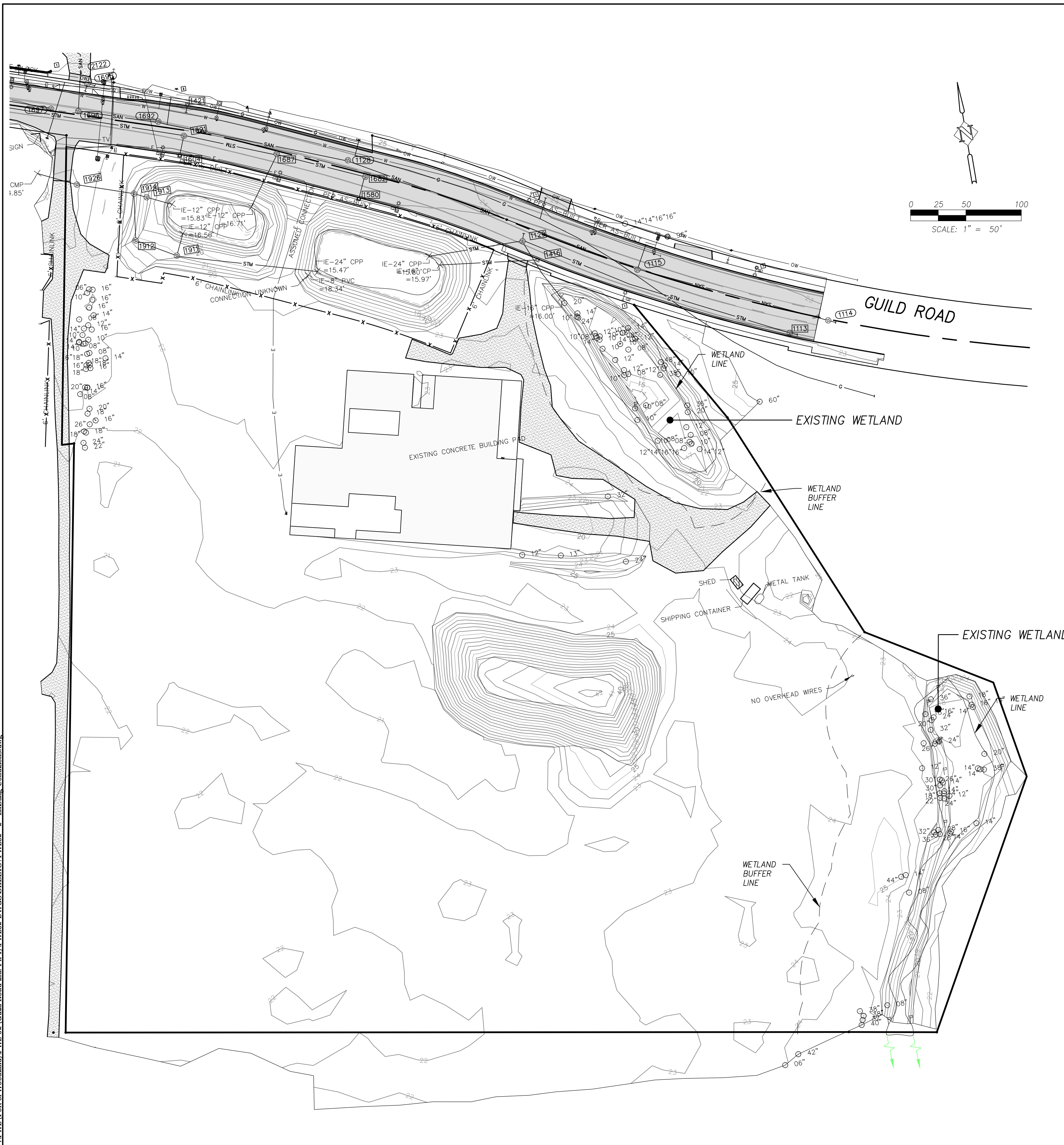


cc: Planning Commission
Applicant
Parties of Record
Mayor
File
Website
City Administrator

ATTACHMENTS

A. Proposed Site Plan

P:\PWL (Port of Woodland)\PWL-02 Guild Road Ind. Ph. DHP\DWG\SHEETS\PWL-02 - 2 - Existing Conditions.dwg



TOPOGRAPHIC LEGEND:

STM	STORM SEWER LINE
SAN	SANITARY SEWER LINE
W	WATER LINE
G	GAS LINE
OW	OVERHEAD WIRE
X	FENCE LINE, TYPE AS NOTED
T	TELEPHONE LINE
E	ELECTRIC LINE
C	CABLE TELEVISION LINE
—	FLOWLINE
—	BUILDING LINE
—	UTILITY LINE IS BELIEVED TO CONTINUE, CONNECTION NOT LOCATED OR UNKNOWN
—	MAJOR CONTOUR - 5' INTERVALS
—	MINOR CONTOUR - 1' INTERVALS
—	UNKNOWN STAND PIPE
—	UNKNOWN UTILITY VAULT
—	UNKNOWN JUNCTION BOX
—	QUIV WIRE
—	STREET LIGHT
—	STREET LIGHT BOX
—	UTILITY POLE
—	UTILITY POLE
—	UNDERGROUND UTILITIES
—	SINGLE POST SIGN
—	GATE POST
—	BOLLARD
—	MAIL-BOX
—	CONIFEROUS TREE (APPROXIMATE DIAMETER BREAST HIGH AS NOTED)
—	DECIDUOUS TREE (APPROXIMATE DIAMETER BREAST HIGH AS NOTED)
—	STUMP - APPROXIMATE DIAMETER ON TOP, OR DBH, IF OVER 4' TALL
—	SEE TOPO/SURVEY NOTES TABLE
—	FOUND MONUMENT, PROTECT.

SS	SANITARY SEWER MANHOLE	SS	SANITARY SEWER MANHOLE
SD	STORM SEWER MANHOLE	SS	SANITARY SEWER MANHOLE
CB	CATCH BASIN	SS	SANITARY SEWER MANHOLE
CI	CURB INLET	SS	SANITARY SEWER MANHOLE
WV	WATER VALVE	SS	SANITARY SEWER MANHOLE
FD	FIRE HYDRANT	SS	SANITARY SEWER MANHOLE
FC	FIRE DEPT. CONNECTION	SS	SANITARY SEWER MANHOLE
WM	WATER METER	SS	SANITARY SEWER MANHOLE
IV	IRRIGATION VALVE	SS	SANITARY SEWER MANHOLE
GW	GAS WARNING SIGN	SS	SANITARY SEWER MANHOLE
PM	POWER METER	SS	SANITARY SEWER MANHOLE
PR	POWER RISER/CABINET/TRANSFORMER	SS	SANITARY SEWER MANHOLE
PS	POWER STAND PIPE	SS	SANITARY SEWER MANHOLE
PE	ELECTRIC BOX	SS	SANITARY SEWER MANHOLE
CR	CABLE TELEVISION RISER	SS	SANITARY SEWER MANHOLE
TR	TELEPHONE RISER	SS	SANITARY SEWER MANHOLE
WF	WETLAND FLAG	SS	SANITARY SEWER MANHOLE
CS	EXISTING CONCRETE SURFACE	SS	SANITARY SEWER MANHOLE
AS	EXISTING ASPHALT SURFACE	SS	SANITARY SEWER MANHOLE
TS	EXISTING TACTILE STRIP	SS	SANITARY SEWER MANHOLE
GS	EXISTING GRAVEL SURFACE	SS	SANITARY SEWER MANHOLE

NOTE: SYMBOLS SHOWN HEREON ARE FOR GRAPHICAL REPRESENTATION PURPOSES AND DO NOT NECESSARILY SHOW SHAPE, SIZE, ROTATION, CONDITION, TYPE, ETC. OF THE ACTUAL PHYSICAL IMPROVEMENTS THAT THEY REPRESENT. CONDITION, TYPE, ROTATION, ETC. MAY VARY AMONGST ITEMS SHOWN BY THE SAME SYMBOL.

TOPO FEATURES NOTES:
 1 *CAUTION* POSSIBLE UNDERGROUND POWER. NO LOCATE PAINT PROVIDED.
 2 *CAUTION* POSSIBLE UNDERGROUND GAS. NO LOCATE PAINT PROVIDED.

UTILITY STATEMENT:
 THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM LOCATE PAINT MARKINGS TIED IN THE FIELD SURVEY AND AS-BUILT DRAWINGS PROVIDED BY UTILITY COMPANIES. THIS SURVEY DOES NOT SHOW ANY PAINT MARKINGS PROVIDED AFTER THE FIELD SURVEY WAS COMPLETED. AS-BUILT DRAWING INFORMATION THAT WAS NOT PROVIDED IS NOT REFLECTED ON THIS SURVEY. AS-BUILT INFORMATION, IF PROVIDED, WAS USED TO IDENTIFY UNDERGROUND PIPE SIZE AND TYPE (IF SHOWN, AND NOT MEASURED). PAINTED SIZE INFORMATION, IF PROVIDED, WAS USED TO IDENTIFY UNDERGROUND PIPE SIZE AND TYPE (IF SHOWN, AND NOT MEASURED). IF NO LOCATE PAINT MARKINGS WERE PROVIDED, AS-BUILT INFORMATION WAS USED TO HORIZONTALLY LOCATE THE UNDERGROUND UTILITIES.
 THIS SURVEY MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE OF ALL SUCH UTILITIES IN THE AREA. THE UNDERGROUND UTILITIES SHOWN MAY NOT BE IN THE EXACT LOCATION AS NOTED ON THIS SURVEY, BUT ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION PROVIDED. MANHOLES OTHER THAN SANITARY AND STORM SEWER WERE IDENTIFIED BY MANHOLE LIDS AND MAY NOT BE LABELED CORRECTLY.

UTILITY LOCATIONS MUST BE VERIFIED BY THE STATE UTILITY NOTIFICATION CENTER IMMEDIATELY PRIOR TO ANY EXCAVATION.
 UTILITY LOCATES WERE REQUESTED 04/19/2017, 04/25/2017, AND 05/15/2017 UNDER TICKET NUMBERS 17122314, 17132925, 17165231.

FOLLOWING IS A LIST OF UTILITY PROVIDERS NOTIFIED:

COMCAST CABLE COMM. MGMT. LLC	(866)873-9735
CASCADE NAT GAS-INGW	(888)522-1130
COWLITZ COUNTY PUD	(360)577-7546
FRONTIER COMMUNICATIONS NW INC	(877)462-8188
BP/OLYMPIC PIPE LINE COMPANY	(425)981-2517
CITY OF WOODLAND	(360)225-7999

* - DENOTES ASBUILT MAPS RECEIVED

MANHOLE LOCATION INFORMATION:
 DUE TO THE HAZARDOUS NATURE AND APPLICABLE OSHA REQUIREMENTS REGARDING CONFINED SPACES, IT IS COMPANY POLICY TO NOT SEND OUR FIELD STAFF INTO UTILITY MANHOLES TO RETRIEVE DEPTH AND SIZE INFORMATION (PIPE SIZES AND DEPTHS ARE OBTAINED THROUGH ABOVE GROUND METHODS). THEREFORE, ANY MANHOLE ELEVATION INFORMATION SHOWN HEREON IS SUBJECT TO AN UNCERTAINTY IN ACCURACY OF PLUS OR MINUS 0.1' OR GREATER (DEPENDING ON DEPTH, SIZE, FLOW, AND CONSTRUCTION OF MANHOLE). MANHOLE PIPE SIZES ARE ALSO SUBJECT TO AN UNCERTAINTY OF TWO INCHES OR MORE (DEPENDING ON DEPTH, SIZE, FLOW, AND CONSTRUCTION OF MANHOLE). IF A HIGHER ACCURACY IS NEEDED, ADDITIONAL TIME, EQUIPMENT, AND PERSONNEL WILL BE REQUIRED TO ENTER THE MANHOLE AND RETRIEVE SAID INFORMATION.
 MANHOLE LOCATIONS SHOWN ARE TO CENTER OF LID, NOT CENTER OF STRUCTURE.

ABBREVIATIONS:
 CPP = CORRUGATED PLASTIC PIPE
 CM = CORRUGATED METAL PIPE
 PVC = PLASTIC PVC PIPE
 ?? = UNABLE TO DETERMINE TYPE OF PIPE (TOO DEEP, ETC.)
 ±±±.8" = APPROXIMATE INVERT ELEVATION

VERTICAL DATUM:
 VERTICAL DATUM IS NAVD 88 BASED NETWORK GPS TIES TO CONTROL ON SITE.
HORIZONTAL DATUM
 HORIZONTAL DATUM IS ASSUMED

BASIS OF BEARINGS:
 BASIS OF BEARING IS ASSUMED SOUTH 80°26'18" EAST BETWEEN FOUND MONUMENTS 106 AND 108.

STORM SEWER NOTES

1113	STORM MANHOLE RIM= 22.97 IE 16" CPP(N)= 17.37 IE 16" CPP(SW)= 17.37 BOTTOM= 17.37	1114	SANITARY MANHOLE RIM= 23.85 IE 12" PVC(NW)= 15.61 IE 12" PVC(SE)= 15.46 BOTTOM= 15.56
1129	STORM MANHOLE RIM= 23.80 IE 12" CPP(SE)= 17.00 IE 12" CPP(N)= 16.85 IE 16" CPP(SE)= 16.45 IE 24" CPP(SW)= 16.15 BOTTOM= 16.30	1115	SANITARY MANHOLE RIM= 23.81 IE 12" PVC(N)= 15.91 IE 12" PVC(NW)= 15.86 IE 12" PVC(SE)= 15.81 BOTTOM= 15.86
1416	CURB INLET OIL TRAP RIM= 24.74 IE 16" CPP(SE)= 21.1 BOTTOM= 19.44 *E IS APPROXIMATE	1128	SANITARY MANHOLE RIM= 24.68 IE 12" PVC(NW)= 16.73 IE 12" PVC(SE)= 16.58 BOTTOM= 16.68
1421	CURB INLET OIL TRAP RIM= 24.38 IE 10" CPP(SW)= 21.9 BOTTOM= 20.38 *E IS APPROXIMATE	1692	SANITARY MANHOLE RIM= 24.50 IE 12" PVC(NW)= 17.25 IE 12" PVC(SE)= 17.10 BOTTOM= 17.20
1580	CURB INLET RIM= 24.45 IE 12" CPP(N)= 20.95 BOTTOM= 19.35	1695	SANITARY MANHOLE FILLED WITH WATER NO FLOW APPEARS CAPPED RIM= 25.06 IE ?? ?(SW)= 9.74 IE ?? ?(NW)= 9.64 IE ?? ?(NE)= 9.54 BOTTOM= 9.56 *E IS APPROXIMATE
1604	CURB INLET RIM= 24.43 IE 12" CPP(N)= 20.73 BOTTOM= 19.13	1696	SANITARY MANHOLE RIM= 24.73 IE 12" PVC(N)= 17.58 IE 12" PVC(SE)= 17.48 BOTTOM= 17.53
1630	CURB INLET OIL TRAP RIM= 24.24 IE 10" CPP(SW)= 21.7 BOTTOM= 20.24 *E IS APPROXIMATE	1697	SANITARY MANHOLE FILLED WITH WATER NO FLOW RIM= 24.83 IE ?? ?(NW)= 9.74 IE ?? ?(NE)= 9.64 BOTTOM= 9.68 *E IS APPROXIMATE
1682	STORM MANHOLE RIM= 24.41 IE 12" CPP(N)= 21.74 IE 12" CPP(NW)= 19.91 BOTTOM= 20.01	2009	SANITARY MANHOLE FILLED WITH WATER NO FLOW RIM= 24.14 IE ?? ?(SE)= 10.24 IE ?? ?(SE)= 10.04 BOTTOM= 10.14 *E IS APPROXIMATE
1687	STORM MANHOLE RIM= 24.74 IE 12" CPP(NW)= 17.04 IE 12" CPP(SE)= 17.04 IE 12" CPP(SW)= 16.94 BOTTOM= 16.99	2122	CLEANOUT RIM= 24.43
1691	STORM MANHOLE RIM= 24.40 IE 12" CPP(SW)= 17.90 IE 12" CPP(NW)= 17.30 IE 12" CPP(SE)= 17.15 BOTTOM= 17.30	4334	SANITARY MANHOLE RIM= 18.73 IE 12" PVC(SE)= 7.51 IE 12" PVC(N)= 7.40 BOTTOM= 7.45
1911	STORM MANHOLE RIM= 20.22 IE 12" CPP(N)= 16.22 IE 24" CPP(SW)= 15.42 BOTTOM= 15.22	4337	SANITARY MANHOLE RIM= 17.47 IE 12" PVC(SW)= 6.72 IE 12" PVC(N)= 6.67 BOTTOM= 7.45
1912	STORM MANHOLE RIM= 20.20 IE 24" CPP(N)= 5.20 IE 24" CPP(N)= 15.00 BOTTOM= 15.10	4448	SANITARY MANHOLE RIM= 17.09 IE 12" PVC(N)= 5.59 IE 12" PVC(SW)= 5.44 IE 12" PVC(NW)= 5.19 BOTTOM= 5.39
1913	STORM MANHOLE RIM= 20.02 IE 12" CPP(N)= 15.82 IE 12" CPP(NW)= 15.82 BOTTOM= 15.72		
1914	STORM MANHOLE RIM= 23.37 IE 12" CPP(SE)= 15.57 IE 24" CPP(N)= 14.57 IE 24" CPP(NW)= 14.57 BOTTOM= 14.57		
1926	STORM MANHOLE RIM= 22.29 IE 24" CPP(SE)= 14.49 IE 36" CM(N)= 14.29 BOTTOM= 14.23		
2009	STORM MANHOLE RIM= 24.09 IE 12" CPP(NW)= 17.84 IE 12" CPP(SE)= 17.74 BOTTOM= 17.79		
4335	CATCH BASIN OIL TRAP RIM= 17.19 IE 8" PVC(W)= 14.44 BOTTOM= 12.79 *E IS APPROXIMATE		
4338	CATCH BASIN OIL TRAP RIM= 17.20 IE 8" PVC(E)= 14.30 IE 12" PVC(W)= 14.34 BOTTOM= 12.42 *E IS APPROXIMATE		
4443	CATCH BASIN OIL TRAP RIM= 17.59 IE 8" PVC(W)= 15.04 BOTTOM= 13.47 *E IS APPROXIMATE		
4444	CATCH BASIN OIL TRAP RIM= 17.58 IE 8" PVC(E)= 14.88 IE 12" PVC(W)= 14.88 BOTTOM= 13.18 *E IS APPROXIMATE		

EXISTING CONDITIONS
GUILD ROAD INDUSTRIAL SITE
 WOODLAND, WASHINGTON

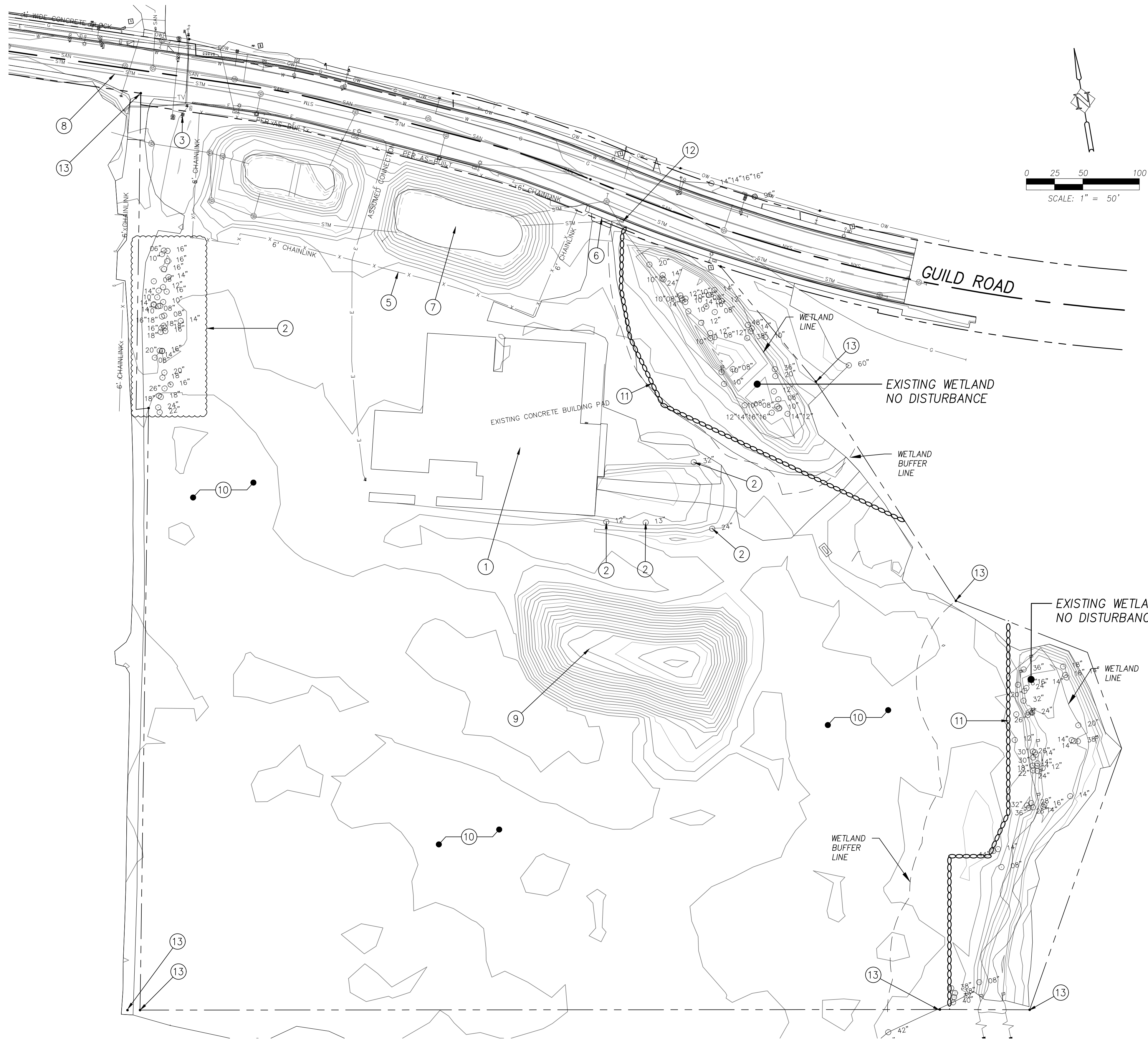
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 1104 Main Street, Suite 100, Vancouver, WA 98660
 Phone: 360.750.1131 www.hhpri.com Fax: 360.750.1141

PRELIMINARY

DESIGNED:	KKV/RAV	DATE:	05/31/2018
DRAWN:	KKV/DBB	DESCRIPTION:	R E V I S I O N S
CHECKED:	RAV	NO.	
DATE:		DATE	

SHEET NO. **2**
 JOB NO. PWL-02

FOR SITE PLAN REVIEW ONLY
NOT FOR CONSTRUCTION



CONSTRUCTION NOTES:

- ① REMOVE EXISTING CONCRETE FOUNDATIONS AND ASSOCIATED STRUCTURES.
- ② REMOVE TREES AND ASSOCIATED ROOTS.
- ③ REMOVE EXISTING WATER METER. CORRDINATE WITH WATER DISTRICT.
- ⑤ REMOVE AND/OR REPLACE EXISTING CHAIN LINK FENCE. SEE POND PLANS.
- ⑥ REMOVE AND REPLACE EXISTING DRIVEWAY APRON. SEE GUILD ROAD PLAN.
- ⑦ REGRADE POND PER POND PLANS.
- ⑧ SAWCUT EXISTING ASPHALT. SEE STREET PLANS.
- ⑨ REMOVE EXISTING SOIL STOCKPILE. CONFER WITH GEOTECHNICAL REPORT FOR SUITABILITY FOR FILL.
- ⑩ STRIP TOPSOIL PER SECTION 5.1 OF THE GEOTECHNICAL REPORT REFERENCED ON THIS SHEET.
- ⑪ INSTALL ORANGE CONSTRUCTION FENCE TO DEMARCAT E NO DISTURBANCE AREAS.
- ⑫ REMOVE EXISTING CURB INLET AND ASSOCIATED SIDEWALK. REPLACE AS NECESSARY. SEE STREET PLAN.
- ⑬ PROTECT OR REPLACE EXISTING MONUMENT.

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DEMOLITION PLAN
GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

Harper Houf Peterson
Righellis Inc.

ENGINEERS • PLANNERS
LANDSCAPE ARCHITECTS • SURVEYORS
205 SE Spokane Street - Suite 200 - Portland, OR 97202
Phone: 503.221.1131 www.hhpri.com fax: 503.221.1171

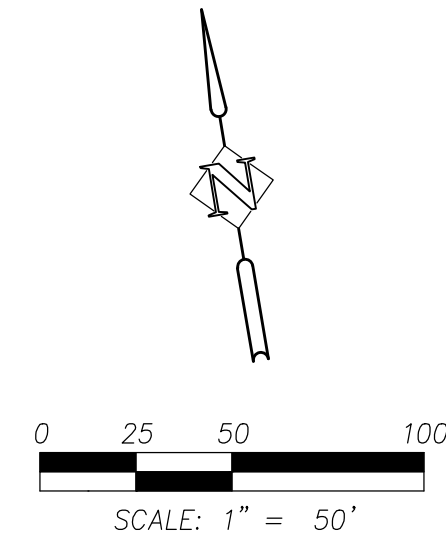
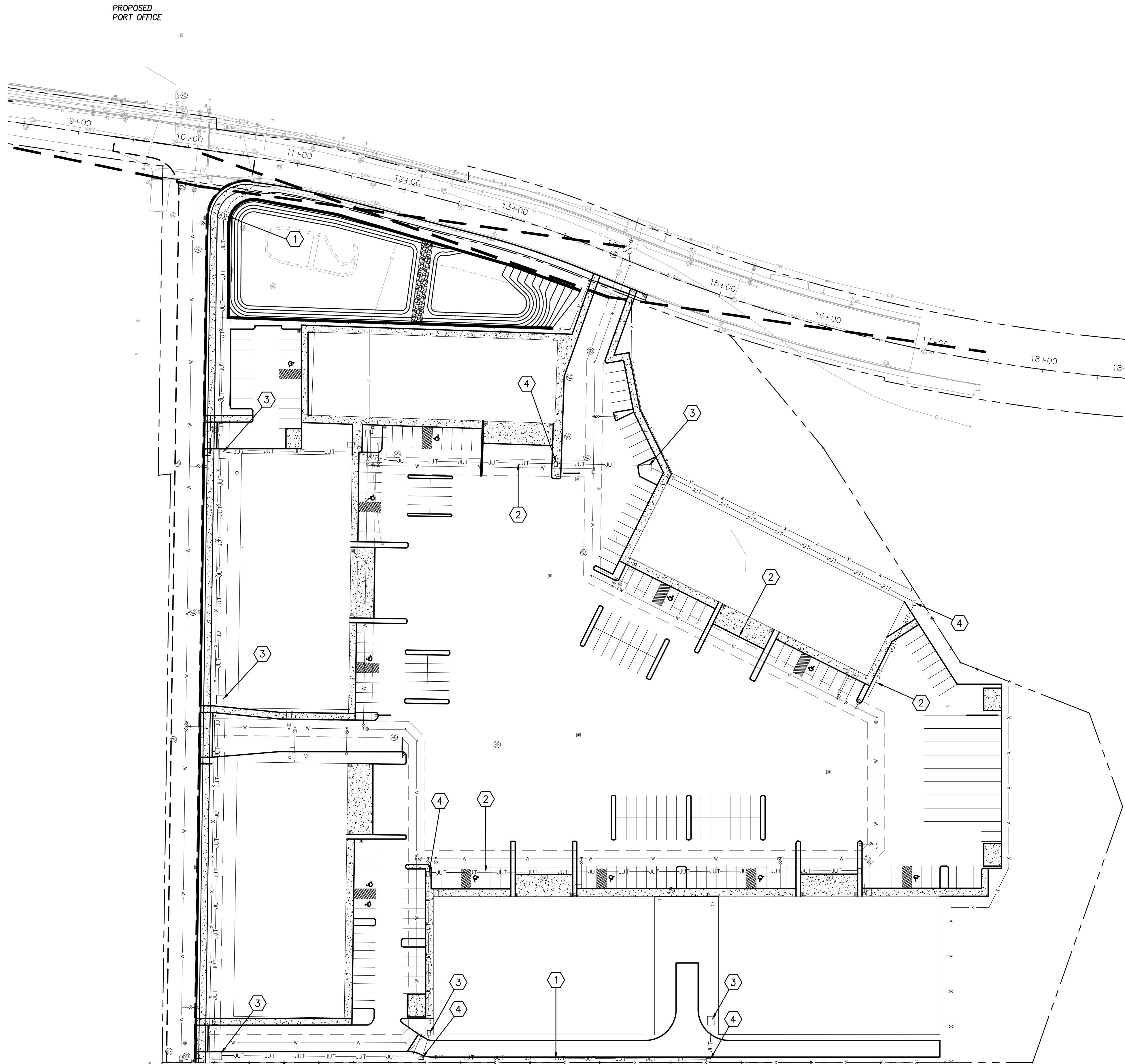
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SHEET NO.
3

JOB NO.
PWL-02

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CONSTRUCTION NOTES:

- ① INSTALL JOINT UTILITY TRENCH PER TYPICAL SECTION.
- ② INSTALL 4" LIGHTING CONDUIT.
- ③ INSTALL POWER VAULT
- ④ INSTALL JUNCTION BOX.

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PRIVATE UTILITY PLAN
 GUILD ROAD INDUSTRIAL SITE
 WOODLAND, WASHINGTON

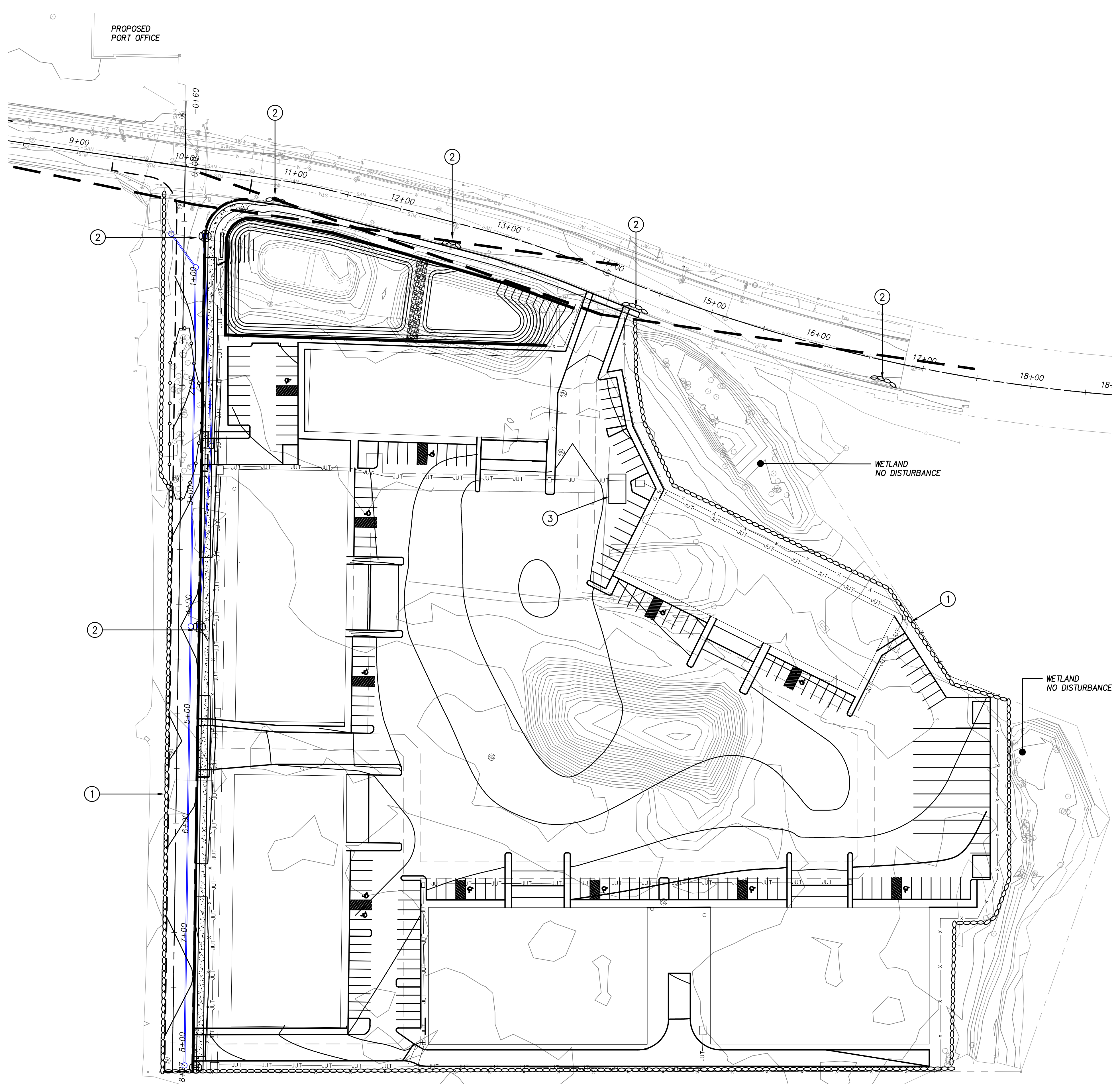
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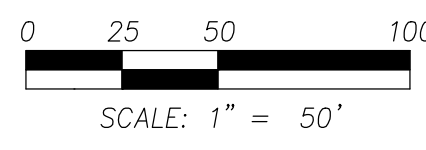
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SHEET NO.	5
JOB NO.	PWL-02

P:\PWL (Port of Woodland)\PWL-02 (Guild Road Ind. Ph. 1)\PWL-02-DWG\SHEETS\PWLO2 - 8 - Erosion Control Plan.dwg



- CONSTRUCTION NOTES:**
- ① INSTALL SEDIMENT FENCE.
 - ② INSTALL INLET PROTECTION.
 - ③ INSTALL CONCRETE WASHOUT.



EROSION CONTROL PLAN
GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

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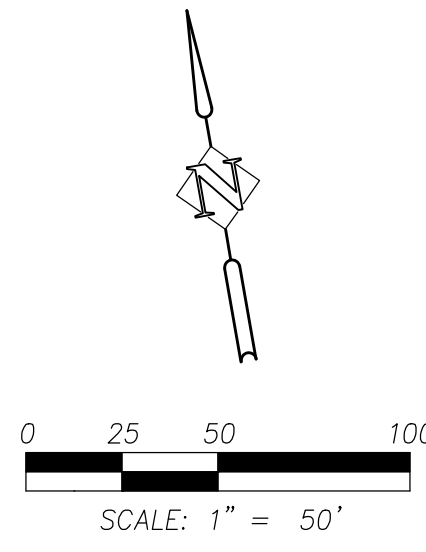
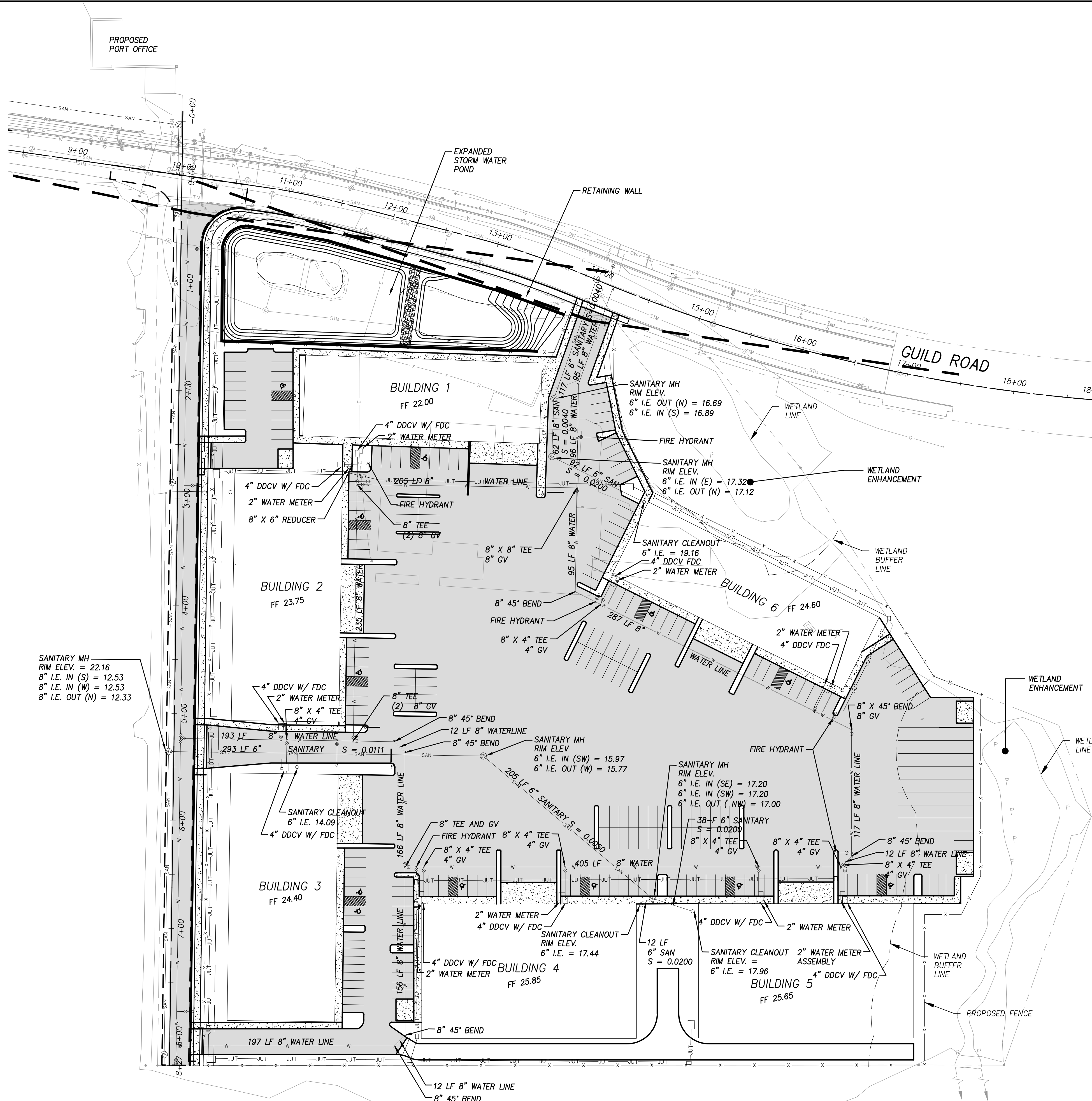
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DATE	NO.	DESCRIPTION	

SHEET NO. 8

JOB NO. PWL-02

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**SANITARY AND WATER PLAN
GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON**

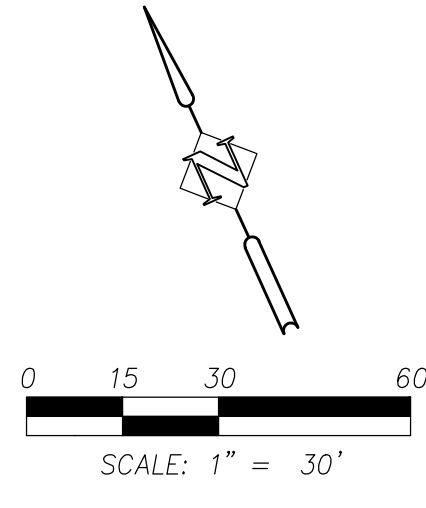
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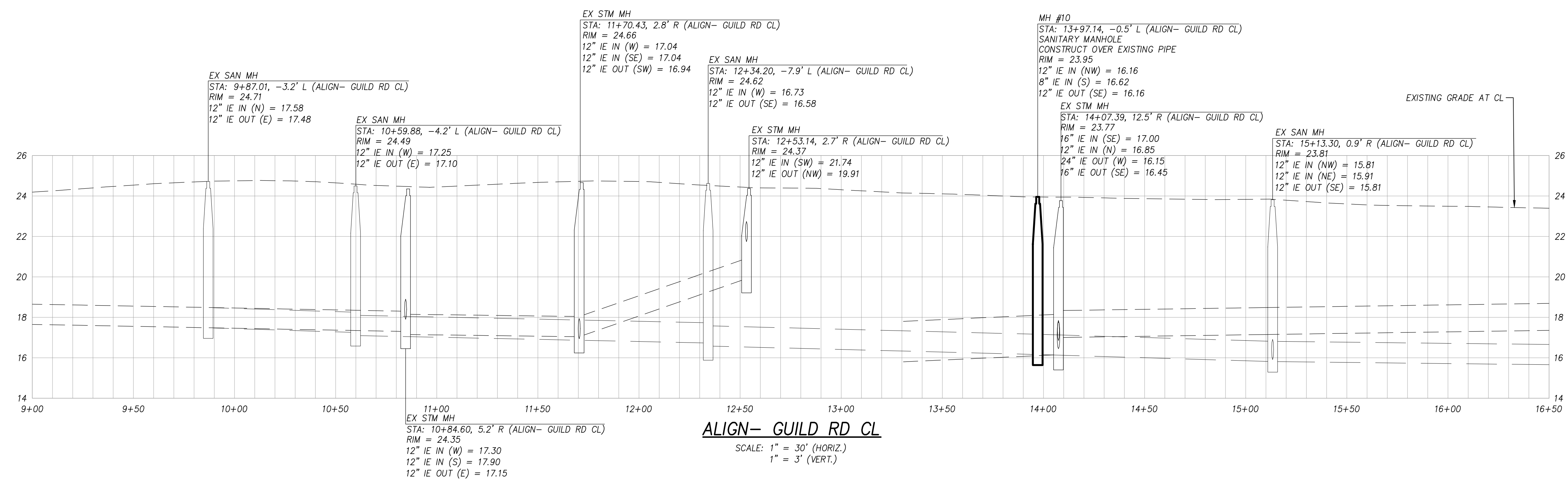
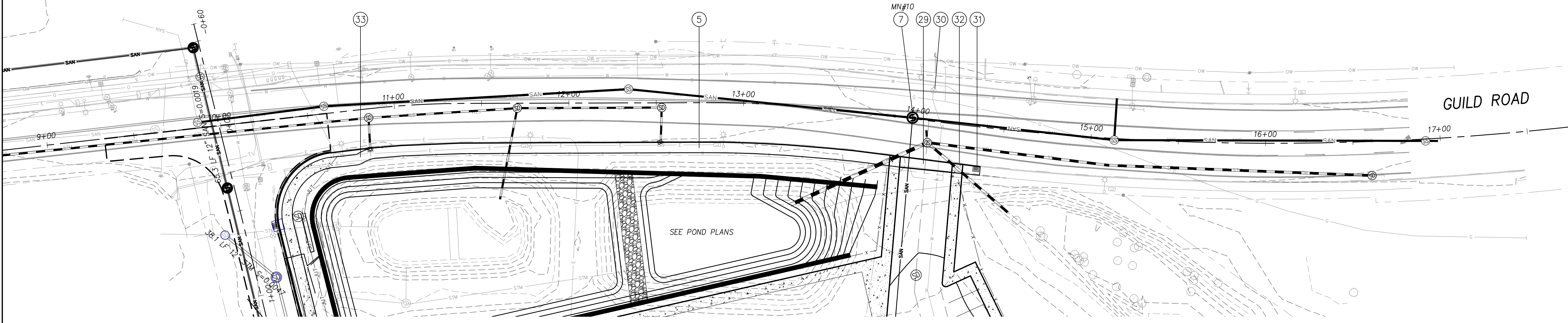
SHEET NO.
9
JOB NO. PWL-02

**FOR SITE PLAN REVIEW ONLY
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CONSTRUCTION NOTES

- ⑤ CONSTRUCT 6" SIDEWALK PER SECTION.
- ⑦ CONSTRUCT SANITARY MANHOLE PER DETAIL.
- ②⑨ STA 14+08.15 CONSTRUCT 30' COMMERCIAL DRIVEWAY.
- ③① STA 14+07 CONNECT TO EXISTING WATER LINE 12" TEE AND BUTTERFLY VALVE.
- ③① STA 14+36.43 INSTALL CURB INLET CONNECT TO EXISTING PIPE.
- ③② REMOVE EXISTING CURB INLET CONNECT TO EXISTING PIPE.
- ③③ TRANSITION SIDEWALK TO CURB TIGHT SIDEWALK.



EX SAN MH
STA: 9+87.01, -3.2' L (ALIGN- GUILD RD CL)
RIM = 24.71
12" IE IN (N) = 17.58
12" IE OUT (E) = 17.48

EX SAN MH
STA: 10+59.88, -4.2' L (ALIGN- GUILD RD CL)
RIM = 24.49
12" IE IN (W) = 17.25
12" IE OUT (E) = 17.10

EX STM MH
STA: 11+70.43, 2.8' R (ALIGN- GUILD RD CL)
RIM = 24.66
12" IE IN (W) = 17.04
12" IE IN (SE) = 17.04
12" IE OUT (SW) = 16.94

EX SAN MH
STA: 12+34.20, -7.9' L (ALIGN- GUILD RD CL)
RIM = 24.62
12" IE IN (W) = 16.73
12" IE OUT (SE) = 16.58

EX STM MH
STA: 12+53.14, 2.7' R (ALIGN- GUILD RD CL)
RIM = 24.37
12" IE IN (SW) = 21.74
12" IE OUT (NW) = 19.91

MH #10
STA: 13+97.14, -0.5' L (ALIGN- GUILD RD CL)
SANITARY MANHOLE
CONSTRUCT OVER EXISTING PIPE
RIM = 23.95
12" IE IN (NW) = 16.16
8" IE IN (S) = 16.62
12" IE OUT (SE) = 16.16

EX STM MH
STA: 14+07.39, 12.5' R (ALIGN- GUILD RD CL)
RIM = 23.77
16" IE IN (SE) = 17.00
12" IE IN (N) = 16.85
24" IE OUT (W) = 16.15
16" IE OUT (SE) = 16.45

EX SAN MH
STA: 15+13.30, 0.9' R (ALIGN- GUILD RD CL)
RIM = 23.81
12" IE IN (NW) = 15.81
12" IE IN (NE) = 15.91
12" IE OUT (SE) = 15.81

GUILD PLAN AND PROFILE
GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

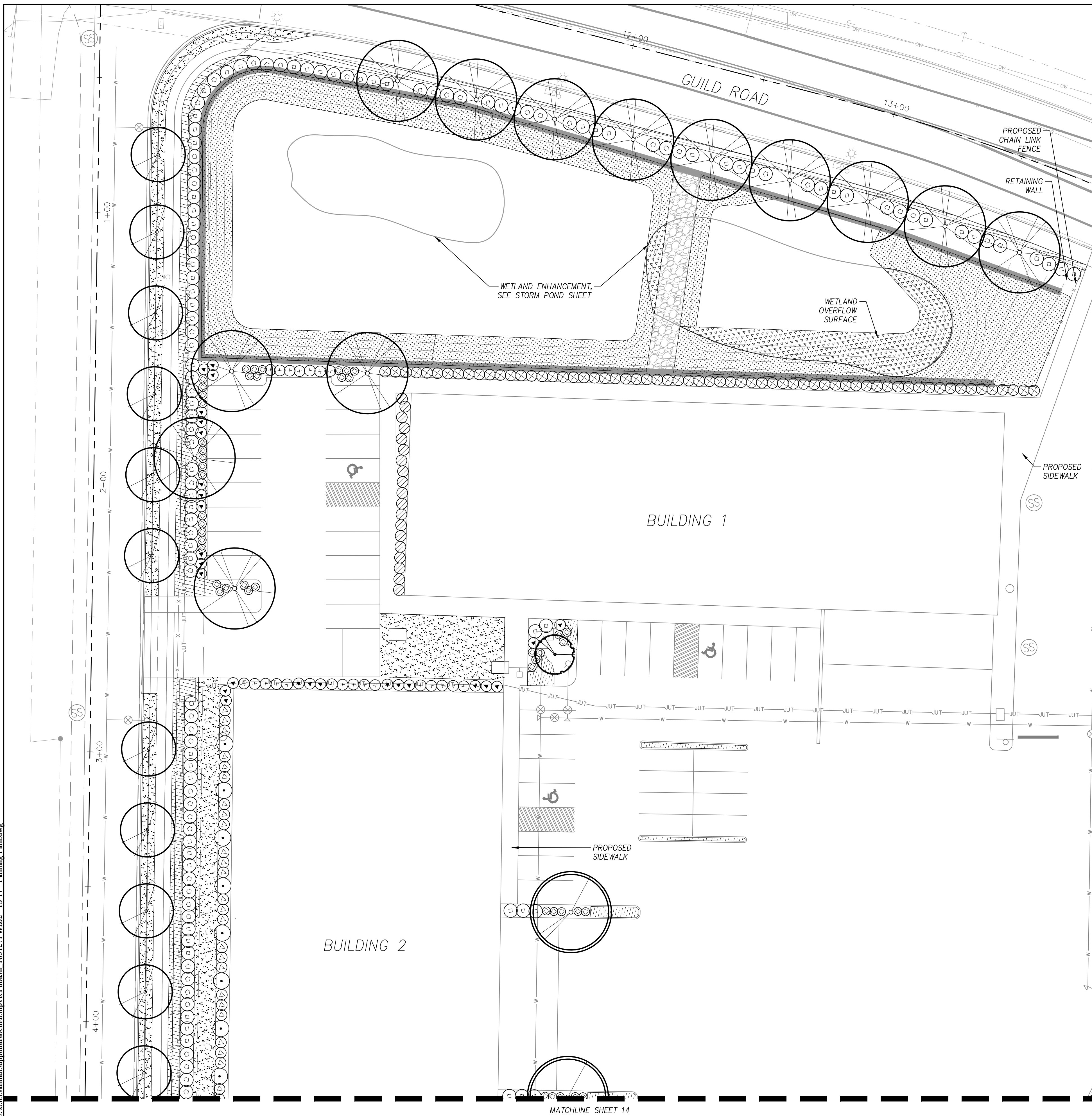
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SHEET NO.
12
JOB NO. PWL-02

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GENERAL PLANTING NOTES:

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH CURRENT CITY OF WOODLAND STANDARDS AND THE CLARK COUNTY BUILDING AND SPECIALTY CODES.
2. INSTALL EROSION CONTROL SYSTEMS IN ACCORDANCE WITH THE CLARK COUNTY STANDARDS PRIOR TO SITE WORK AND LANDSCAPE INSTALLATION.
3. CONTRACTOR SHALL MARK AND PROTECT ALL UTILITIES, SITE FEATURES, AND VEGETATION TO REMAIN IN PLACE.
4. CONTRACTOR SHALL REMOVE ALL WEEDS AND INVASIVE SPECIES SHALL BE REMOVED PRIOR TO PLANTING OR SEEDING
5. PRIOR TO PLANTING, CONTRACTOR SHALL TEST ON-SITE SOILS FOR SOIL FERTILITY BY A CERTIFIED TESTING LAB. IF NECESSARY, BACKFILL SOILS FOR TREE PITS, SHRUB AND GROUND COVER AREAS SHALL BE AMENDED AS RECOMMENDED BY THE SOIL FERTILITY REPORT.
6. ALL SEEDED AREAS SHALL BE STRIPPED OF VEGETATION, SCARIFIED AND RECEIVE 4" OF TOPSOIL PRIOR TO APPLICATION OF SEED.
7. ALL GROUND COVER BEDS SHALL BE SCARIFIED 12" BELOW FINISHED GRADE AND HAVE 12" OF TOPSOIL ADDED TO BRING BACK TO FINISHED GRADE PRIOR TO PLANTING.
8. LANDSCAPE INSTALLATION SHALL INCLUDE PROVISION OF AN AUTOMATIC IRRIGATION SYSTEM TO SUSTAIN LANDSCAPE PLANTINGS, MEETING LOCAL AND STATE BUILDING CODES.
9. PLANT MATERIAL INSTALLED SHALL CONFORM IN SIZE AND GRADE TO THE "AMERICAN STANDARD FOR NURSERY STOCK" CURRENT EDITION.
10. THE QUANTITIES OF PLANT MATERIALS SHALL BE AS DETERMINED BY THE CONTRACTOR IN ACCORDANCE WITH THE SPECIFIED SPACING OR LOCATION ON THE PLAN. MATERIAL QUANTITIES SHOWN ON THE PLAN ARE FOR CONTRACTOR CONVENIENCE ONLY AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO INSTALLATION. SURPLUS OR SHORTAGES OF PLANT QUANTITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
11. LANDSCAPE CONTRACTOR SHALL WATER PLANTINGS FOR THE DURATION OF ONE-YEAR WARRANTY PERIOD AFTER INSTALLATION AND GUARANTEE ALL PLANTINGS TO BE ALIVE AND IN SATISFACTORY HEALTH.

NOTES:

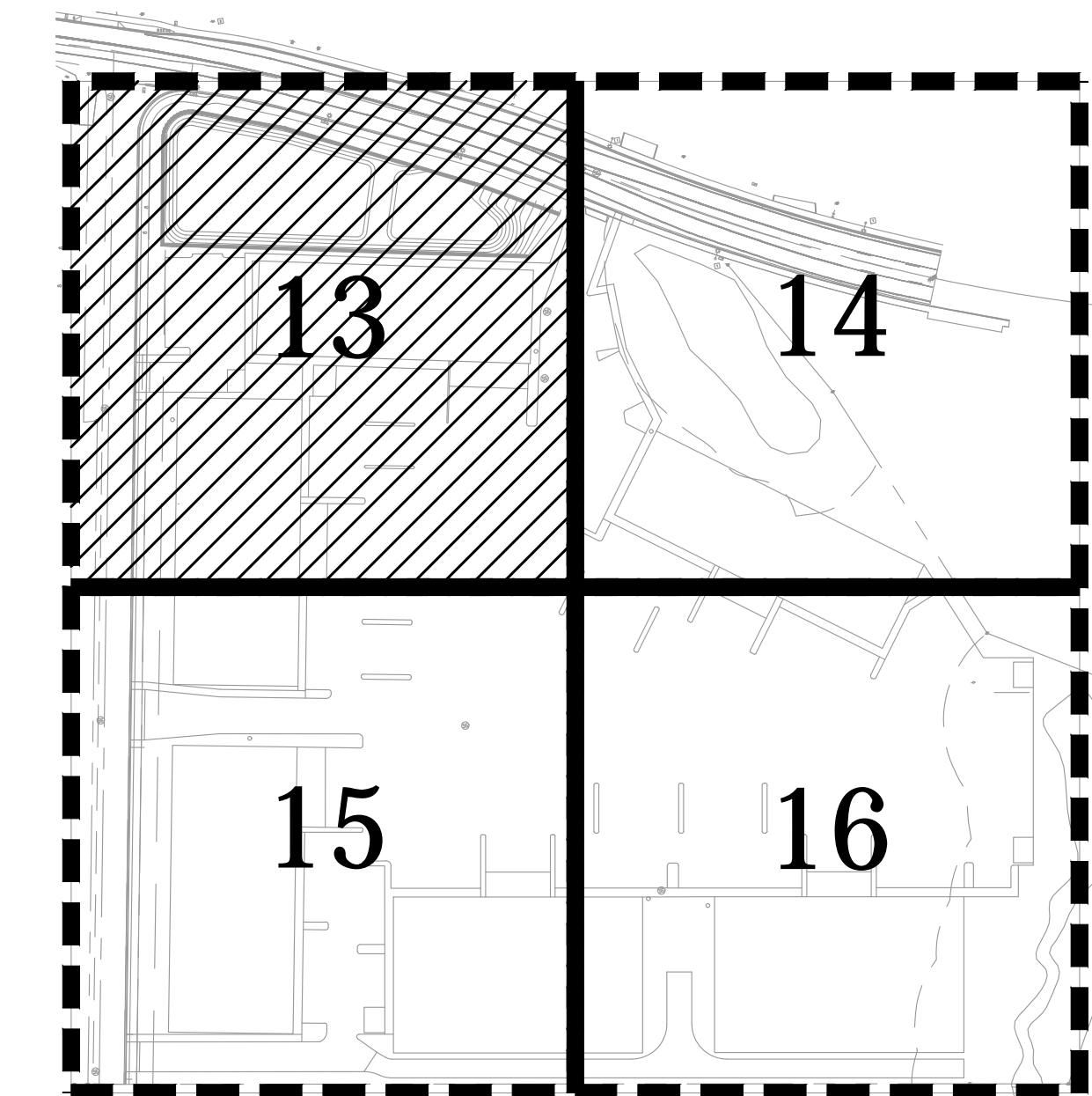
PLANTING SCHEDULE SEE SHEET 17

CODE REQUIREMENTS:

TOTAL SITE	525,601 SF
LANDSCAPE REQUIREMENT (10%)	52,560 SF
WETLAND ENHANCEMENT AREA	51,895 SF
EAST SETBACK AREAS	9,425 SF
WEST SETBACK AREAS	13,493 SF
SOUTH PERIMETER LANDSCAPE AREA	4,138 SF
INTERIOR LANDSCAPING AREAS	3,071 SF
PARKING LOT LANDSCAPE AREAS	6,409 SF
TOTAL LANDSCAPE AREA*	88,431 SF (17%)

*EXCLUDES SWALE AREA

KEY MAP



FOR SITE PLAN REVIEW ONLY
NOT FOR CONSTRUCTION

GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

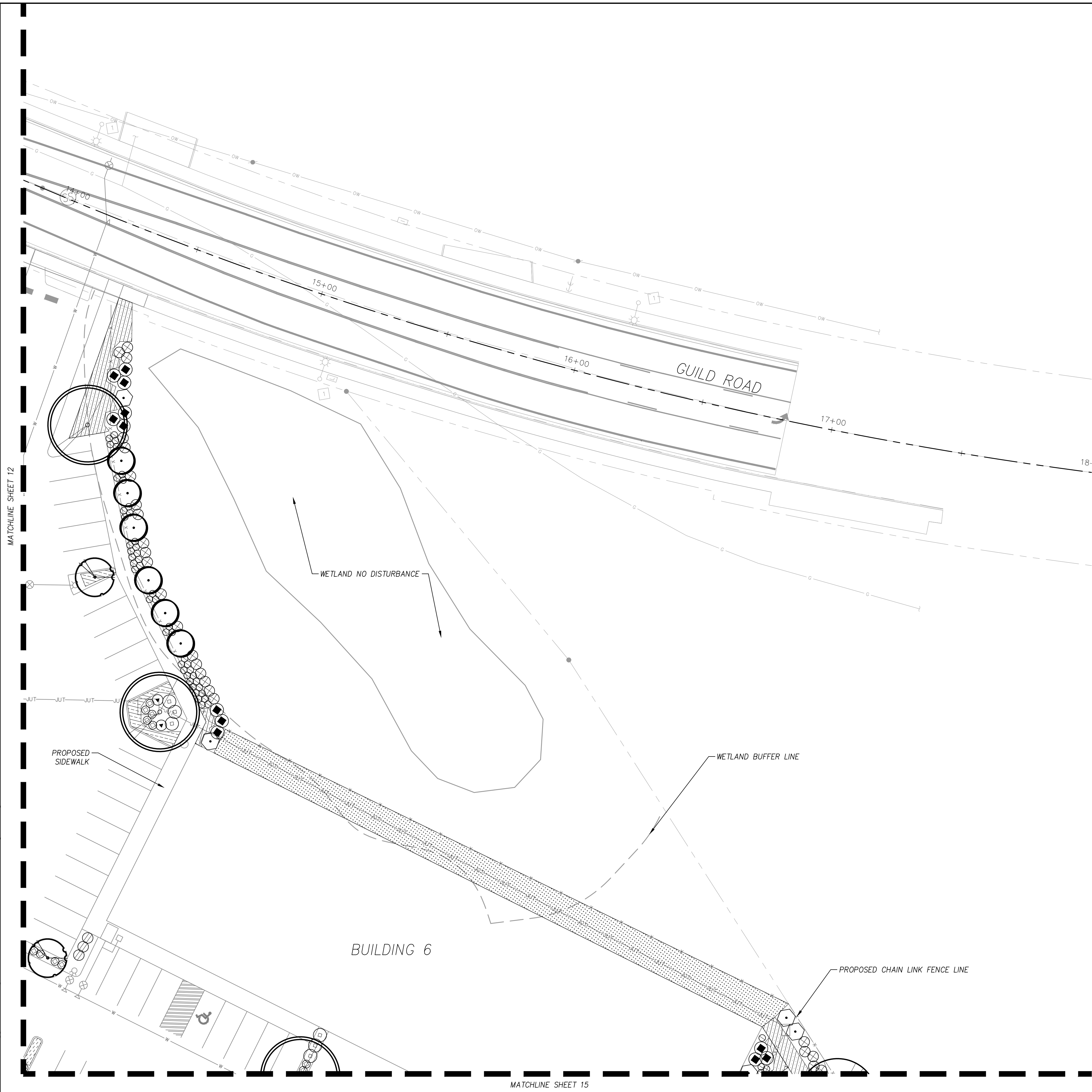
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DATE:	05/31/2018

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JOB NO.	PWL-02

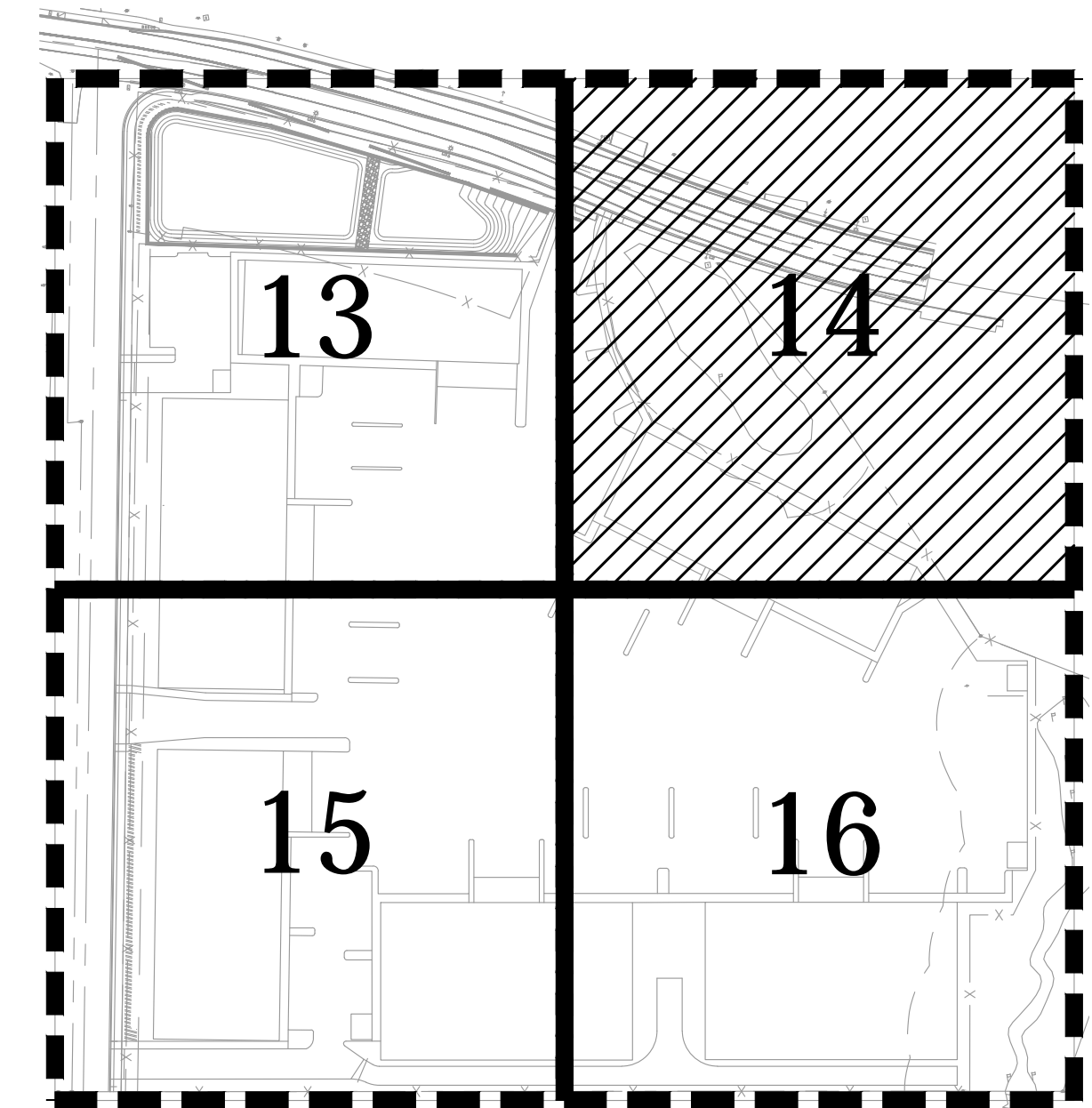
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NOTES:

PLANTING SCHEDULE SEE SHEET 17

KEY MAP



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GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

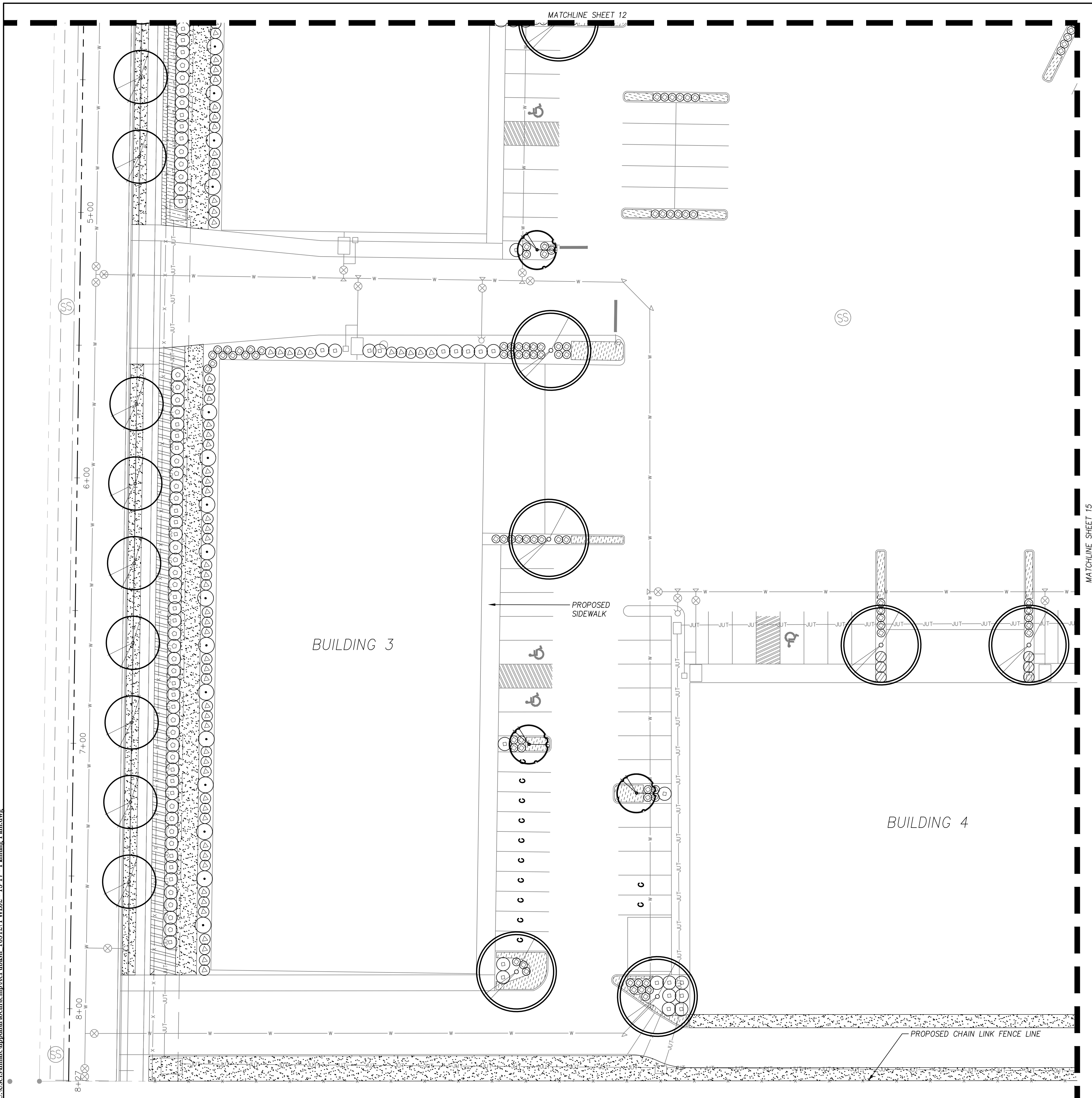
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JOB NO.	PWL-02

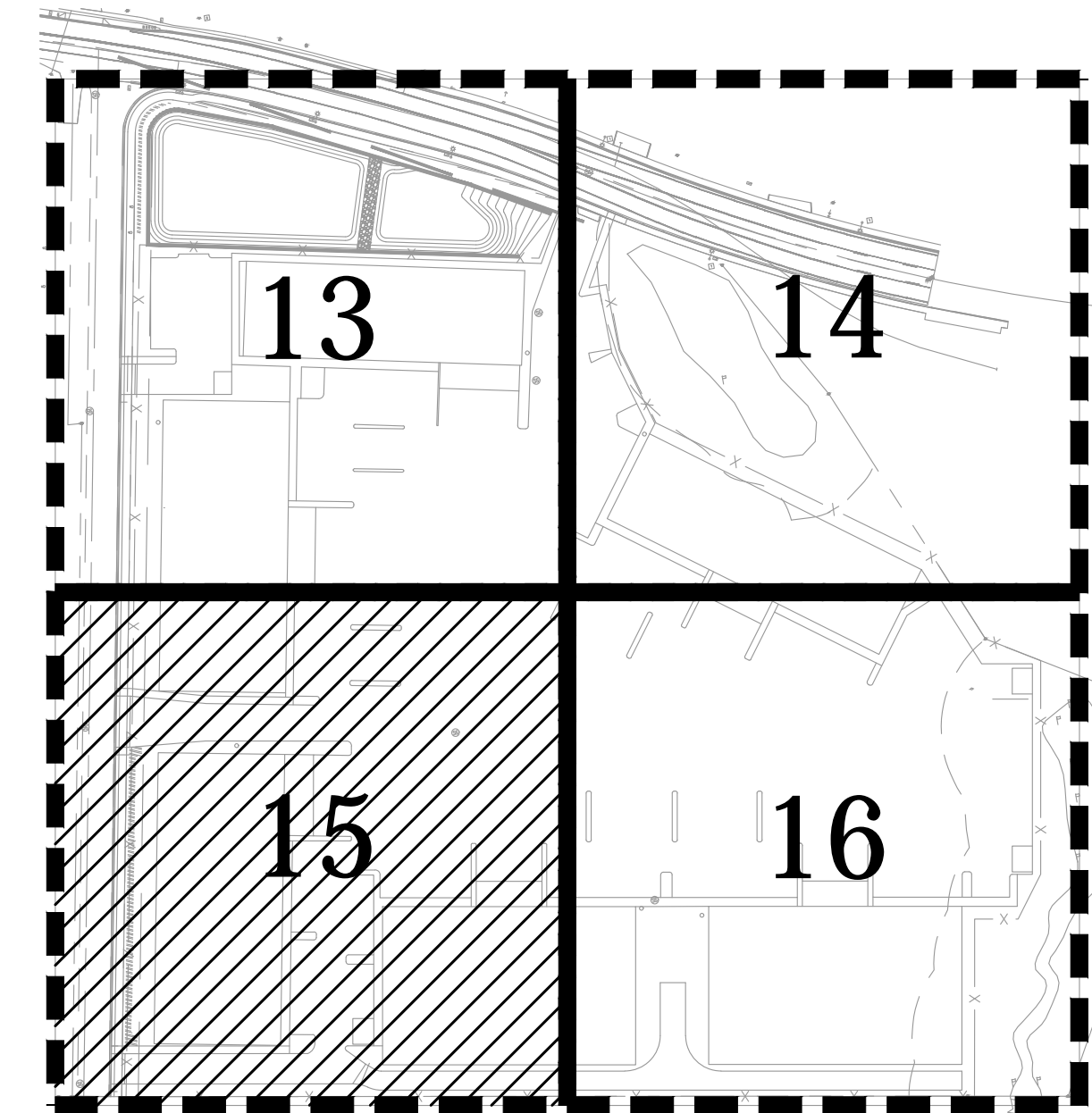
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NOTES:

PLANTING SCHEDULE SEE SHEET 17

KEY MAP



FOR SITE PLAN REVIEW ONLY
NOT FOR CONSTRUCTION

Harper Houf Peterson Righellis Inc.
ENGINEERS + PLANNERS
LANDSCAPE ARCHITECTS + SURVEYORS
205 SE Spokane Street, Suite 200, Portland, OR 97202
Phone: 503.221.1171 www.hhp.com fax: 503.221.1171

PRELIMINARY

DESIGNED:	KKV/RAV
DRAWN:	KKV/JDB
CHECKED:	RAV
DATE:	05/31/2018

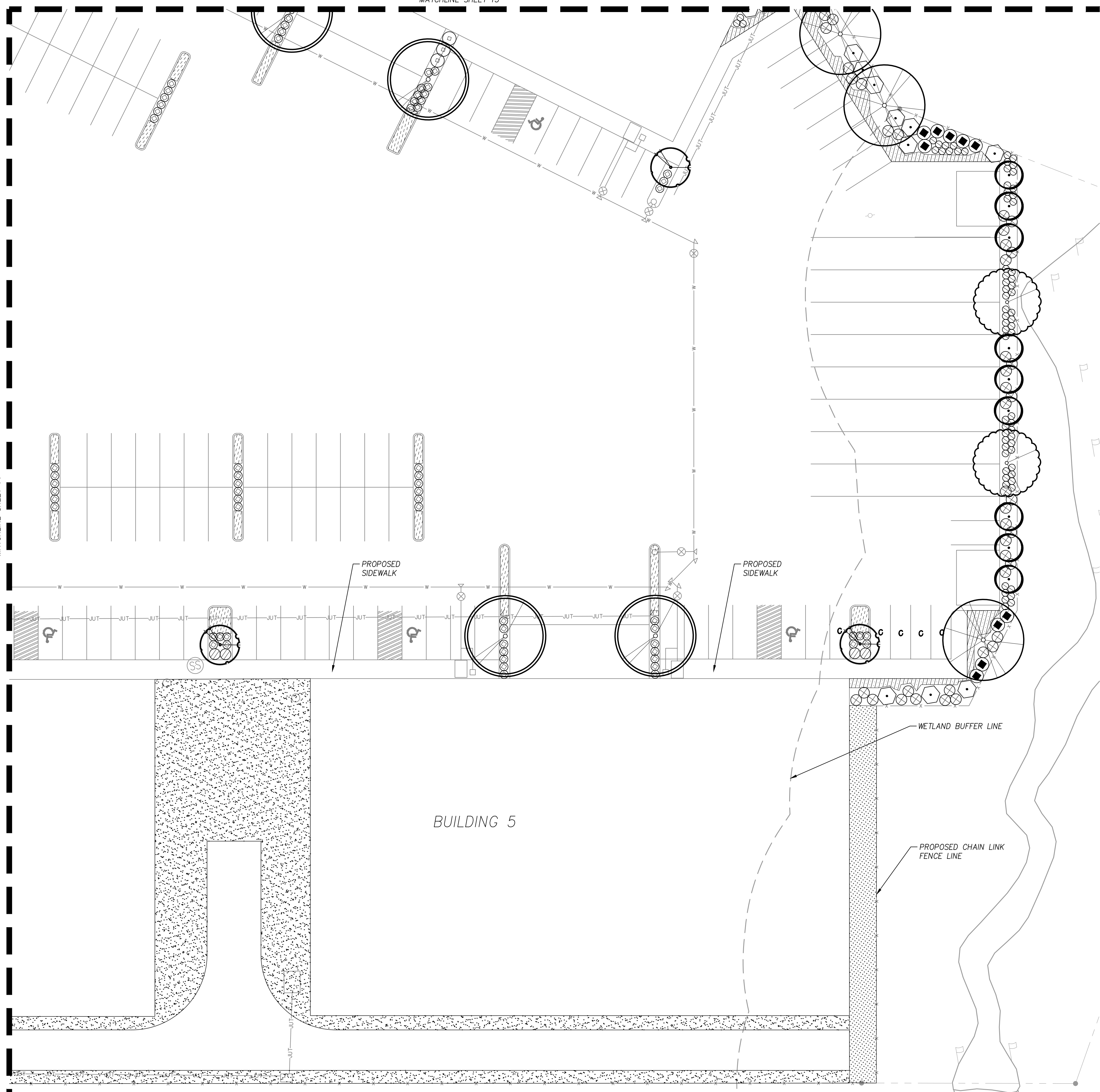
SHEET NO.	
OF	
JOB NO.	PWL-02

GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

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MATCHLINE SHEET 14

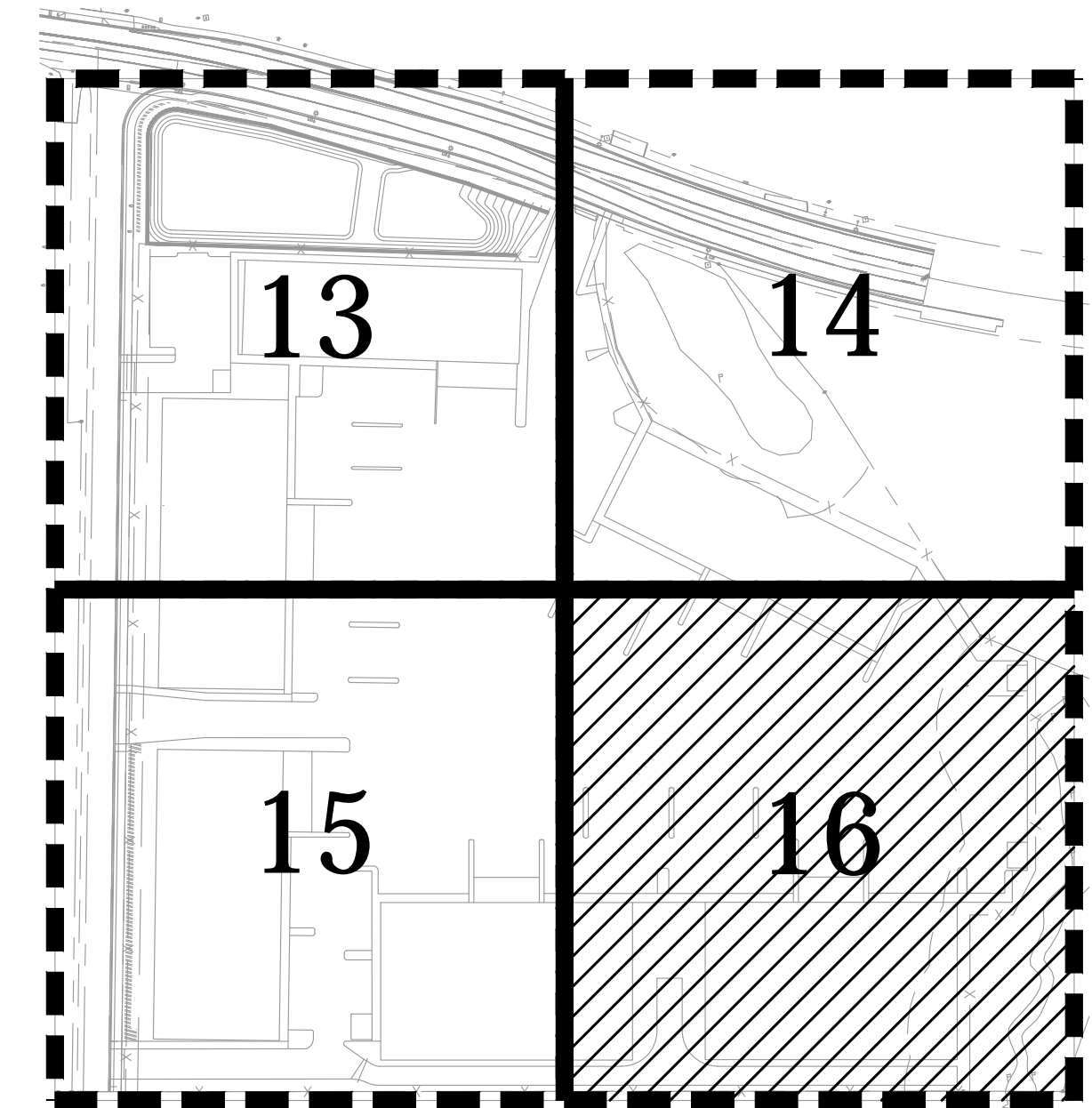
MATCHLINE SHEET 13



NOTES:

PLANTING SCHEDULE SEE SHEET 17

KEY MAP



FOR SITE PLAN REVIEW ONLY
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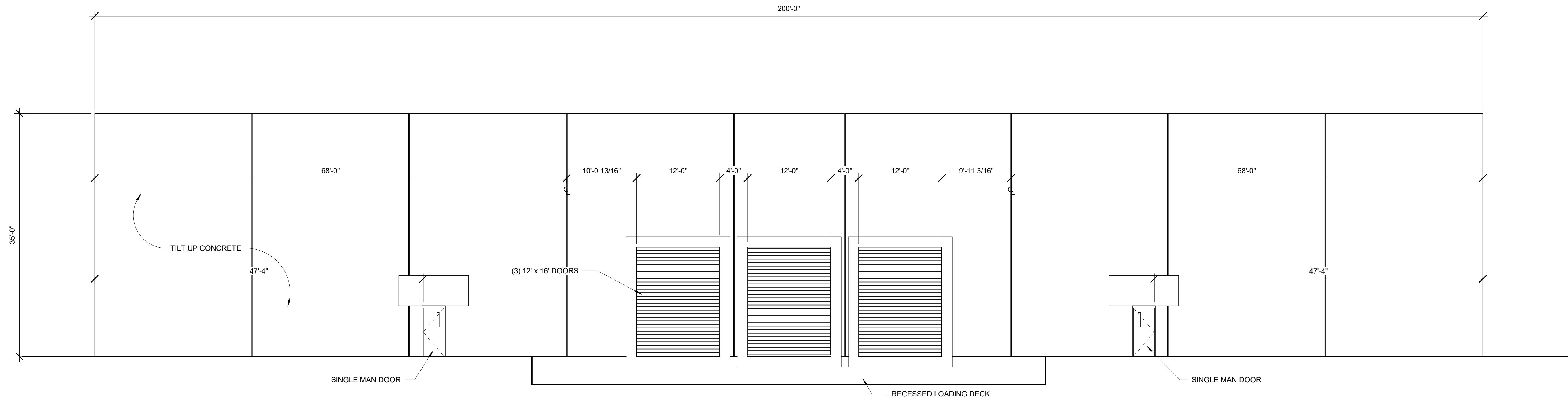
GUILD ROAD INDUSTRIAL SITE
WOODLAND, WASHINGTON

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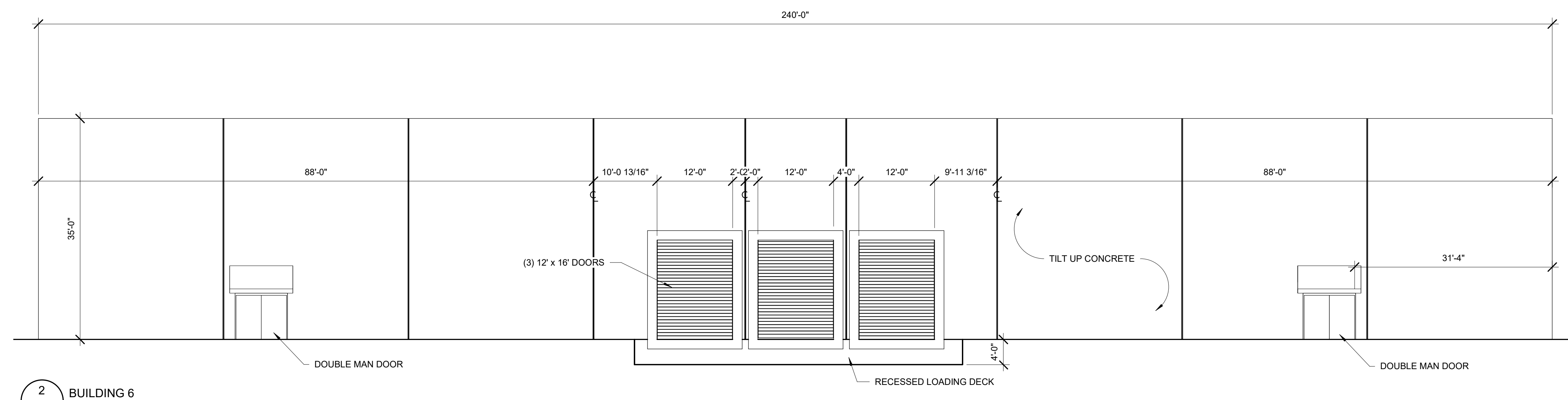
PRELIMINARY

DESIGNED:	KKV/RAV
DRAWN:	KKV/JDB
CHECKED:	RAV
DATE:	05/31/2018

SHEET NO.	
OF	
JOB NO.	PWL-02



1 BUILDING 4 & 5
 19 1/8" = 1'-0"



2 BUILDING 6
 19 3/32" = 1'-0"

DESIGNED:	Designer
DRAWN:	Author
CHECKED:	Checker
DATE:	03/29/18

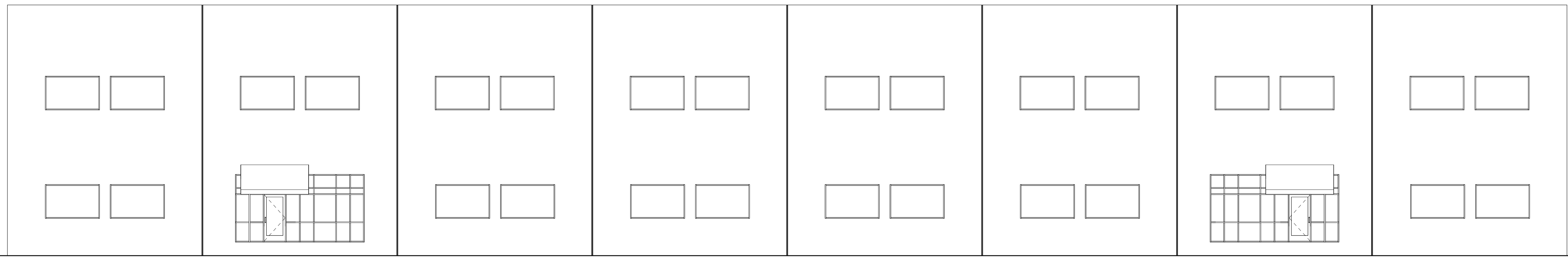
DATE	NO.	DESCRIPTION

SHEET NO.

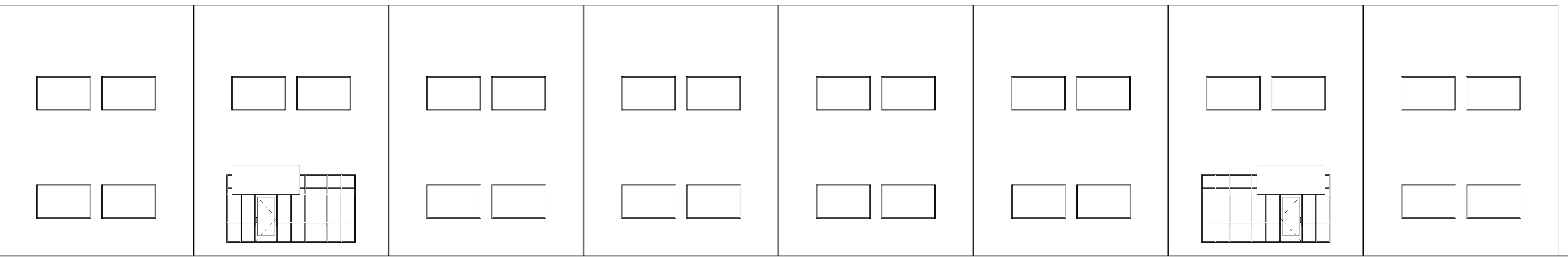
19

JOB NO. PWL-02

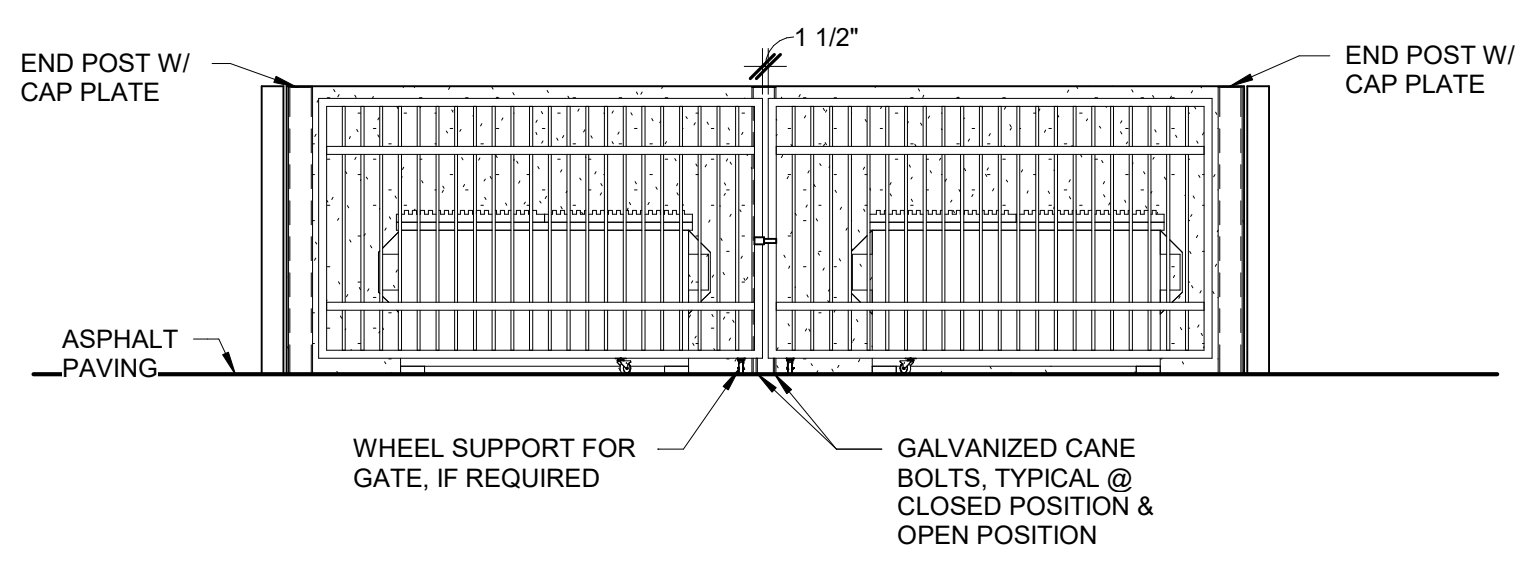
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1 BUILDING 2 STREET ELEVATION
 20 1/8" = 1'-0"



2 BUILDING 3 STREET ELEVATION
 20 1/8" = 1'-0"



3 GARBAGE AREA ELEVATION
 20 1/4" = 1'-0"

DESIGNED:	Designer
DRAWN:	Author
CHECKED:	Checker
DATE:	06/15/18

DATE	NO.	DESCRIPTION

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